Husband, John

From: @smec.com]
Sent: Tuesday, 13 May 2014 11:35 AM
To: Hancock, Patrick
Cc: HPDE; Cargill, James; Husband, John;
Subject: 2014 05 13 VAR 014 - Modifications to Concrete Works at Bridge Approaches (HPD2-O-0290-E) (09.0)
Attachments: HPD2-O-0280-L VAR 014 - Modifications to Concrete Works at Bridge Approaches.pdf; HPD2-V14-AT 28-4-14.pdf; VO.14 - Marked up sketch of items.pdf

Pat/John,

Please find attached my assessment of Hewatt's Variation Claim No. 14 for works associated with the modifications to the design at the bridge approaches. I have also attached Hewatt's original claim.

The total claim for VO.14 is for

I have made a note in my assessment that these works potentially delayed Hewatt's critical path, however Hewatt have not claimed any EOTs (or delay costs). As per Clause 35.5, as 28 days have elapsed since the occurrence of the delay, Hewatt will not be entitled to any EOT Claims (and therefore any delay costs).

Given the above, it is my assessment that the is a fair and reasonable assessment of the value of the variation.

Please let me know if you have any questions.

Regards,

Suite 2, Level 1
243 Northbourne Avenue
Lyneham Canberra ACT 2602
Telephone +61 2 6234 1900
Facsimile +61 2 6234 1966
www.smec.com

29/04/2014

Shared Services Procurement
PO Box 818
DICKSON, ACT 2602

Our ref: HPD2-O-0280-L / 09.0

Attention: **Patrick Hancock**
Senior Project Officer

Contract No: 2013.21048.110
Horse Park Drive Extension – Burrumarra Avenue to Mirrabei Drive

SMEC's Assessment of VAR 014 - Modifications to Concrete Works at Bridge Approaches

Patrick,

SMEC received Variation Claim No. 14 from Hewatt (letter Ref: HPD2-VO14-AT) on 28 April 2014. The Variation Claim seeks payment for the additional works associated with the construction of concrete protection slabs, minor concrete retaining walls and concrete footpath with railing footing at the bridge. The lump sum price submitted by Hewatt is for

Provided below is a summary of the background to the design errors initiating the Variation, details on each construction element of the Variation and SMEC's assessment of the submitted price by Hewatt. A drawing showing of each of the elements is also attached for reference.

BACKGROUND

The change in the design which has initiated VAR 014 was developed as a consequence of two design errors associated with the design of the bridge and also the adjacent earthwork formation. These design errors included:

A. **No provision for protection of service conduits adjacent to the ends of the approach slabs**
The service conduits for the shared trench are located within the footpath section of the bridge deck and approach slab concrete pours (as per drawing M110549-S08 & S09). The cover over the conduits through the bridge is approx. 100mm. There was no provision in the original design for the protection of these conduits at the ends of the approach slabs prior to achieving the typical shared trench formation detailed in drawing C11108-018 (with minimum covers as per Table 3.1 in Section 3 of the ACT Standard Specification).

This error was raised by the Contractor in RFI084A, to which the solution was to construct concrete pavement (PT6) for the trunk path above the conduits until necessary cover was achieved. This initiated the concrete path as detailed in Item 3 below.

However, within 2-3m at either end of the approach slabs, the pavement depth for PT6 was not achievable. Therefore a concrete encased section was required until sufficient depth could be achieved. This created the need for the concrete encasement referenced in Item 1 below.

B. Discrepancies between the bridge design and the earthworks formation designs at either end of the approach slabs

The design for the bridge deck and approach slab specifies a barrier kerb on either side of the carriageway with a footpath at 3% cross-fall towards the carriageway. However, the design for the earthworks formation (immediately prior to the approach slabs) specifies flush kerb with verges at 2% cross-fall towards Moncrieff. This discrepancy creates a difference in levels between the bridge and the earthworks within the verges.

The difference in level has prevented the original pedestrian railing footing to be constructed. Therefore, the footing for the pedestrian railing on the Moncrieff side of the bridge has been incorporated into the footpath and concrete encasement required as part of the solution to the issues in Part A. On the Taylor/Casey side of the bridge, a strip footing has been used, which also acts as a small retaining wall to address the change in levels between the bridge and the adjacent earthworks.

1. ***Concrete protection slab at either end of approach slabs (Moncrieff side of bridge) to provide sufficient protection to the service conduits through the bridge-deck***

The works involved included:

- Preparation of foundations and base material;
- Extension of conduits for encasement;
- Forming and pouring for the concrete works (including adding expansion joints surrounding the guardrail posts);
- Finishing of the concrete and stripping formwork;
- Fixing the pedestrian railing once completed.

Price Submitted:

SMEC's Assessment:

The price submitted by Hewatt is fair and reasonable for the works required to be undertaken. According to records held by SMEC, the labour, equipment and materials being claimed are generally an accurate reflection on what was carried out. Hewatt have not claimed any time or costs associated with setting out the railing and works to the original design. No Extensions of Time or delay costs have been claimed for these works.

2. ***Minor retaining wall incorporating footings for the pedestrian railing at either end of the approach slabs (Casey side of bridge) to address discrepancies between the earthworks and bridgeworks designs***

The works involved included:

- Removal of previously completed works as per the original design;
- Preparation of foundations and base material;
- Set out for final levels;
- Forming and pouring for the concrete works;
- Finishing of the concrete and stripping formwork;
- Fixing the pedestrian railing once completed.

Price Submitted:

SMEC's Assessment:

The price submitted by Hewatt is fair and reasonable for the works required to be undertaken. According to records held by SMEC, the labour, equipment and materials being claimed are generally an accurate reflection on what was carried out. Whilst the cost of the subcontractor's work appears high compared with Items 1 & 3, SMEC notes that the subcontractor's rates for Items 1 & 3 are quite reasonable and the Variation in general does not include costs for a number of items carried out by the contractor (eg. removal of previously completed works). No Extensions of Time or delay costs have been claimed for these works.

3. ***Concrete footpath on Moncrieff side of bridge (incorporating pedestrian railing footing) at either end of the concrete protection slab (see Item 1) to provide protection above the service conduits until adequate cover could be achieved***

   The works involved included:

   - Removal of previously completed works as per the original design;
   - Preparation of foundations and base material (care had to be taken around the service conduits);
   - Set out for final levels;
   - Forming and pouring for the concrete works (including thickened reinforced concrete edge to act as railing footing);
   - Finishing of the concrete and stripping formwork;
   - Fixing the pedestrian railing once completed.

   Price Submitted:

   SMEC's Assessment:

   The price submitted by Hewatt is fair and reasonable for the works required to be undertaken. According to records held by SMEC, the labour, equipment and materials being claimed is generally an accurate reflection on what was carried out. No Extensions of Time or delay costs have been claimed for these works.

CONCLUSION

It is SMEC's assessment that the prices submitted by the Contractor for each of the constructed elements within Variation Claim 14 are fair and reasonable. The Variation Claim also includes a sum for the payment of survey services used by Hewatt to construct the works (and provide WAE records). The 12 hours of survey services submitted is generally in accordance with the services that SMEC observed being carried out on-site.

It is noted that the design errors considerably impacted on the progress and 'momentum' of the constructed works and may have contributed to delays in the installation of the kerb and guardrail. Hewatt have not claimed any Extension of Time or delay costs for these delays. As 28 days have now elapsed since the time at which the potential delay has occurred, Hewatt are not able to submit a claim as per Clause 35.5 (b) of AS 2124-1992. A note attached to SMEC's Variation response to Hewatt will specify that there will be no Extension of Time associated with the Variation.

Therefore, SMEC's assessment under Clause 40.5(c) of AS2124 is that the value of VAR 014 is to be

Please contact the undersigned if you request any further details.

Yours sincerely,

Superintendent's Representative
CC: EDD, John Husband

HEWATT
CIVIL ENGINEERING CONTRACTOR

ABN: 39 120 060 095
P.O. Box 94
HALL ACT 2618
Phone: 02 6227 5694
Fax: 02 6227 5687
E-Mail: info@hewatt.com.au
www.hewatt.com.au

Our Ref: HPD2-VO14-AT

28 April 2014

The Superintendent's Representative
SMEC Australia Pty Ltd
Suite 2, Level 1
243 Northbourne Avenue
LYNEHAM, ACT, 2602

Attention:

CONTRACT REF: 2013.21048.110
HORSE PARK DRIVE EXTENSION – BURRUMARRA AVE to MIRRABEI DRIVE
VARIATION 14 – Modifications to Concrete Works on Bridge Approaches and Departures

Dear Rob,

Further to RFI 97A response dated 17/2/14, the revisions to the design require Hewatt to execute additional work. In accordance with Clause 40.1 (d) of AS2124 as amended, the additional works of protection slabs, modified kerbs and modified footpaths adjacent to the bridge are a variation.

In accordance with Clause 40.5 (c) we provide our lump sum price of $29,606.45 (exl GST).

The attached spreadsheet summarises details the prices for the separate portions of the additional works.

Item	Description	Qty	Unit	Rate	Total
A	Footpath / Service Protection Slab	1	Item		
B	Special Kerb	1	Item		
C	Bridge Path with Railing Footing	1	Item		
Other	Survey (modelling, set out, WAE)	12	hr		
				TOTAL (ex GST)	

Also attached is a copy of the invoice from detailing variation prices. Additional information on materials prices can be forwarded if required.

We trust the above meets with your approval. Please contact the undersigned should you have any queries or require any additional information.

Regards

Engineering Manger
Hewatt Pty Ltd

Attach : Variation Details spreadsheet pages A, B, C (3 pages)
Invoice
Part responses to RFIs 97 and 101 (5 pages)

VARIATION DETAILS	Variation No	14

24/04/14

HEWATT

HORSE PARK DRIVE 2

Part 1 of 3

ITEM	DESCRIPTION	UNIT	QUANTITY	RATE	Total
	Footpath / Service Protection Slab				
	Construct services protection slab as per Section 2 of RFI 97 sketch attachments. Slab is on the approach and departure from the right hand side of the bridge (ie 2 areas)				
1	Supply 32MPa Concrete	m3	6	$	
2	Supply reinforcement	ea	2	$	
3	Supply miscellaneous materials (for service conduits)	ea	2	$	
4	Labour (adjust conduits, extend sleeves, prepare base)	hr	16	$	
5	r (subcontract form, fix, pour, finish)	item	1	$	
	SUB-TOTAL				

Add MARGIN @

Total excluding GST:

GST

Grand Total Including GST:

VARIATION DETAILS | Variation No 14

24/04/14

HEWATT

HORSE PARK DRIVE 2

Part 2 of 3

ITEM	DESCRIPTION	UNIT	QUANTITY	RATE	Total
	Special Kerb				
	Construct special flush kerb as per details provided in RFI 97 sketch attachments on approach and departure from the left hand side of the bridge (ie 2 areas)				
1	Supply 32MPa Concrete	m3	3	$	
2	Supply reinforcement	ea	2	$	
3	(subcontract form, fix, pour, finish)	item	1	$	
	Earthworks - excavation, base preparation, regrading				
1	5t excavator	hr	8	$	
2	Labour	hr	8	$	
3	Small tools	day	1	$	
4	Supply DGB20	t	2	$	
	SUB-TOTAL				

Add MARGIN @

Total excluding GST:

GST

Grand Total including GST:

VARIATION DETAILS | Variation No 14

24/04/14

HEWATT

HORSE PARK DRIVE 2

Part 3 of 3

ITEM	DESCRIPTION	UNIT	QUANTITY	RATE	Total
	Bridge path with railing Footing				
	Construct bridge path with railing footings as detailed in RFI 97 sketch attachments on approach and departure from the right hand side of the bridge (ie 2 areas)				
1	Supply 25MPa Concrete	m3	7	$	
2	Supply reinforcement	ea	2	$	
3	(subcontract form, fix, pour, finish)	item	1	$	
	Earthworks - excavation, base preparation, regrading				
1	5t excavator	hr	8	$	
2	Labour	hr	8	$	
3	Small tools	day	1	$	
	SUB-TOTAL				

Add MARGIN @

Total excluding GST:

GST

Grand Total Including GST:

Tax Invoice

Hewatt Earthworks Pty Ltd
PO Box 94
Hall ACT 2618

26 / 3/ 2014

Invoice No

Project: Horse Park Drive – Stage 2

Day Docket 477 128m mowing strip @
Day Docket 507 6 hours @
Bridge Relieving Slabs
Special Flush Kerbs
Bridge Path incl footing & handrail

Sub-Total

GST

TOTAL

Please direct deposit in the following account

This is a Payment Claim made under the Building & Construction Industry Security of Payment ACT (NSW) 1999 and the Building & Construction Industry (Security of Payment Act) 2009

Request For Information

HEWATT

Project: ***Horse Park Drive Extension – Burrumarra to Mirrabei*** **Project File No.:.....**

RFI Title: Transition of footpath and kerb from Bridge & Approach Slab

Issued To: **Brown –** **Copied:** SMEC –
Hewatt –

Originator		Discipline:	Bridgeworks,footpaths,kerbs	RFI No:	R 097 A
Email	@smec.com	Issue Date:	21/1/14	Issue Time:	1:00pm
Mobile		Req By Date	24/1/14	Req Time	12pm
Email	@brownconsulting.com.au				

Reference	See attached sketches, mark-ups and drawings	Spec.

Could you please provide clarification/solutions to the following issues with the design at the transition to the bridge and approach slabs? SMEC has provided options for potential solutions to the problems. See attached documents and sketches for further details.

1. **Drainage:** surface drainage captured by BKG over bridge discharging concentrated flow at the point that the BKG transitions to SFK. At this point the cross-fall of the path has to be away from the road (which is not possible under the current design – due to the pedestrian fencing);
2. **Cross fall of footpath:** footpath cross-fall needs to transition from 3% towards the bridge to 2% away from the bridge over 10m (as per RFI013A)
3. **Footpath levels:** the levels of the footpath are governed by the level of the base of the pedestrian handrail on one side and the top of the kerb on the other side for the 8m either side of the approach slabs (meaning a cross-fall from the fence to the SFK of 9.8% at some locations).
4. **Levels of pedestrian fencing do not coincide with the levels of earthworks**: the design for the earthwork formation does not tie in flush to the base of the pedestrian fence (ie. level of path beneath fence). Therefore, the top of footings for the fence posts will sit ~300mm above the earthworks level.

Response From: ı (Brown)/ (SMEC) **Date/Time of Response:** 28/01/2014

The details of the footpath and kerb transition adjacent to the approach slabs on the western and eastern ends of the bridge (for the low side, adjacent to the westbound carriageway) are to be as per the following:

- BKG to be extended from the approach slab to the extents of the pedestrian railing fence beyond the approach slab (approx. 8.6m from approach slab). This will maintain an approx. 3% cross-fall of the footpath towards the road. Beyond this point the BKG is to transition to an SFK over 10m. The footpath is to transition from 3% crossfall towards the road to a 2% crossfall away from the road over this 10m. A revised model has been provided with this response.
- An approx. 2m section from the approach slab is to be concrete encased until minimum cover is achieved to service utilities. Reinforcement details for this section are shown in attached sketch (HPD2 Footpath Transition Details Past...)
- The footings for the pedestrian railing are to be incorporated into the footpath as per the sections shown in the attached sketch. Note, the depth of concrete for the concrete relieving slab (RFI023A) is to be 125mm thick (not 100mm thick).
- Given the presence of the PD1 fence at the top of the stone-pitched batter (RFI065A), a 1:4 (V:H) batter is acceptable from the top of the mowing strip to where the batter will meet the level of the footpath. Note: for this distance from the stone-pitching a 500mm deep concrete encasement will have an exposed vertical face.
- Additionally (though not shown in the attached sketch) any exposed face should SL81 or N12 bars at 200 centres each way with 40mm cover to limit shrinkage cracking.

Please raise a Variation for any additional works associated with the above solution.

Reference Old Documents or New Document/s Issued:

HPD2 Footpath Transition Detail Past Bridge 240114_RLedit; Transitions at bridge.12da

Request answered: ☐ Yes ☐ No if No, raise a new RFI with same number and next suffix letter

RFI Signed off:
Contractor PM _____ **Date:** _____

Request For Information

HEWATT

Project File No.:.....

Project: *Horse Park Drive Extension – Burrumarra to Mirrabei*

Issued To: SMEC – - (SR)

Copied: SMEC –
SMEC –

Originator		Discipline:	Civil	RFI No:	101 A
Email	@hewatt.com.au	Issue Date:	5/2/2014	Issue Time:	4:00pm
Mobile		Req By Date	6/2/2014	Req Time	4:00pm
Email	@SMEC.com (SR)				
Reference	RFI 97 response dated 28/1/2013	Spec.			

Please refer to the response we received to RFI 97.

This is specific to the right hand side of the bridge to incorporate the protection slab and path with the handrail footing.

Please confirm the handrail footing on the left hand side of the bridge is as per Drg M110549-S13/B.

Response From: (SMEC) **Date/Time of Response:** 17/2/2014

The footing for the handrail shall be incorporated into a 300mm wide Mowing Strip/Special Flush Kerb that will be aligned with the base of the handrail posts. The MS/SFK should have a layer of L8TM with 50mm cover at the top and bottom of the MS/SFK, as per SFK detail in dwg C11108-013. Joints are to be spaced as per dwg C11108-013.
Please refer to attached sketch for details.
This MS/SFK shall form the edge of the decomposed red granite at this location, as per RFI071A.
An inspection of the alignment and steel placement will be required from the Superintendent prior to pouring.

Reference Old Documents or New Document/s Issued:
RFI101-RLSK02

Request answered: ☐ Yes ☐ No if No, raise a new RFI with same number and next suffix letter

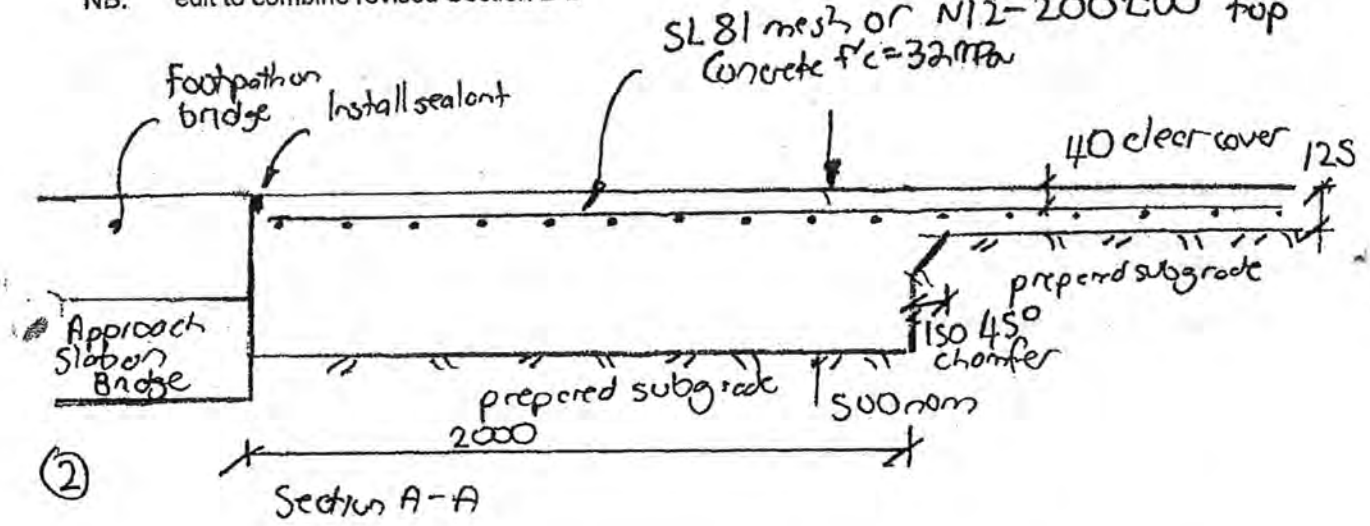
RFI Signed off:
Contractor PM ..... Date: .....

V14

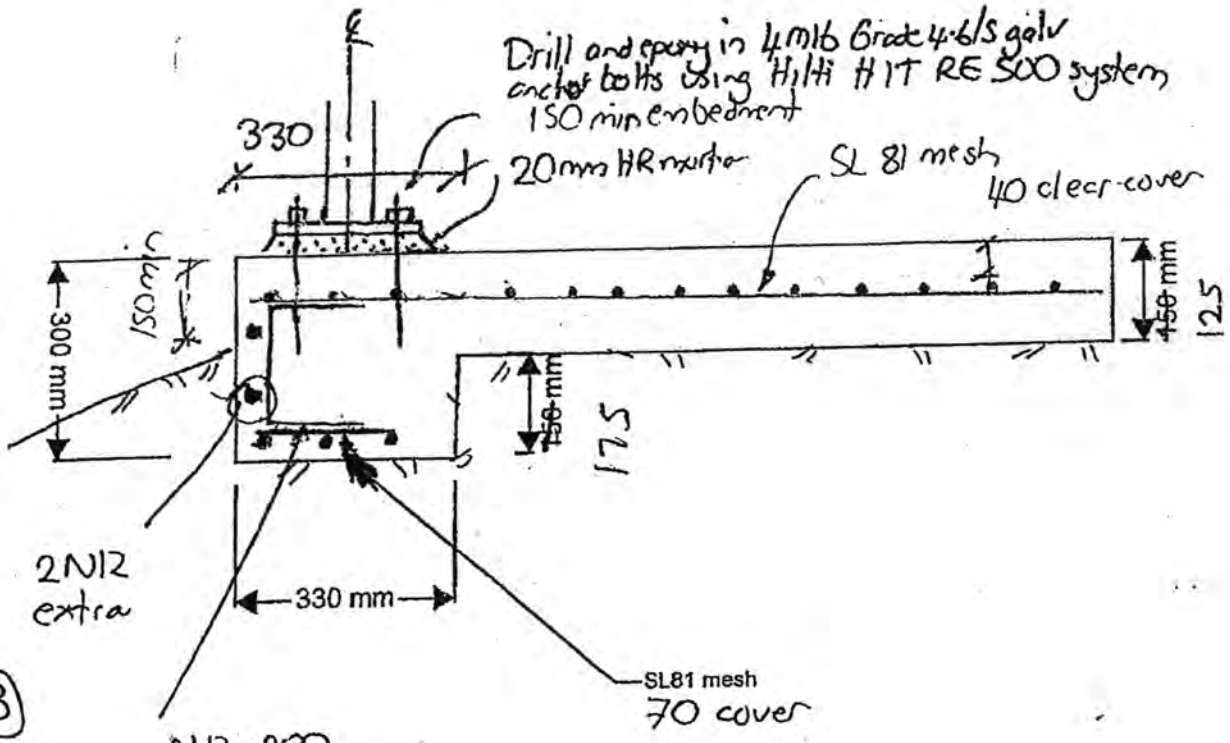
BROWN
Smart Consulting

Title	Horsepark Drive 2 – RFI		Page A2
Prepared by	DIB	Job/Project No. M110549	Date 24/1/14

NB: edit to combine revised Section B-B

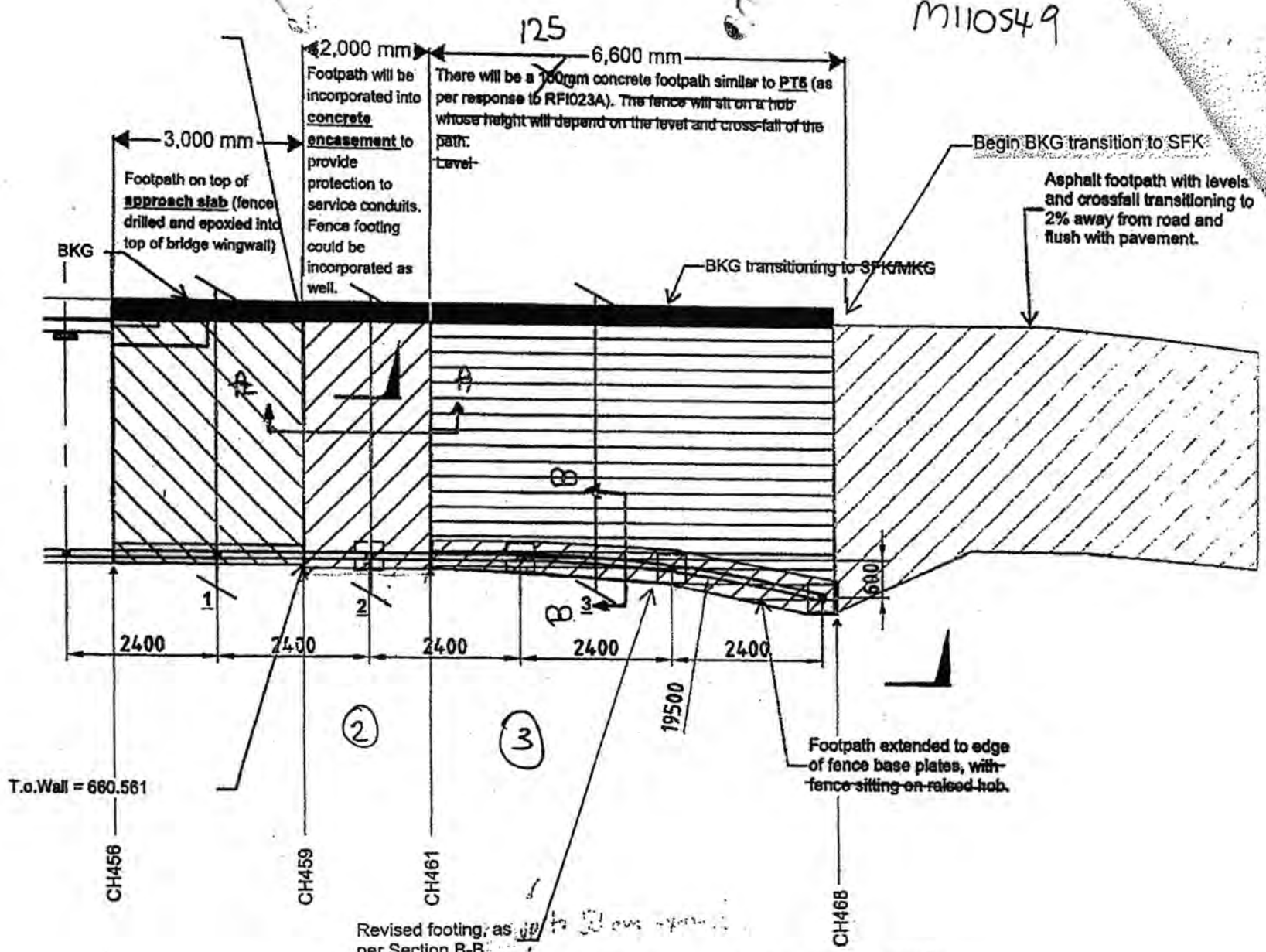


(2)

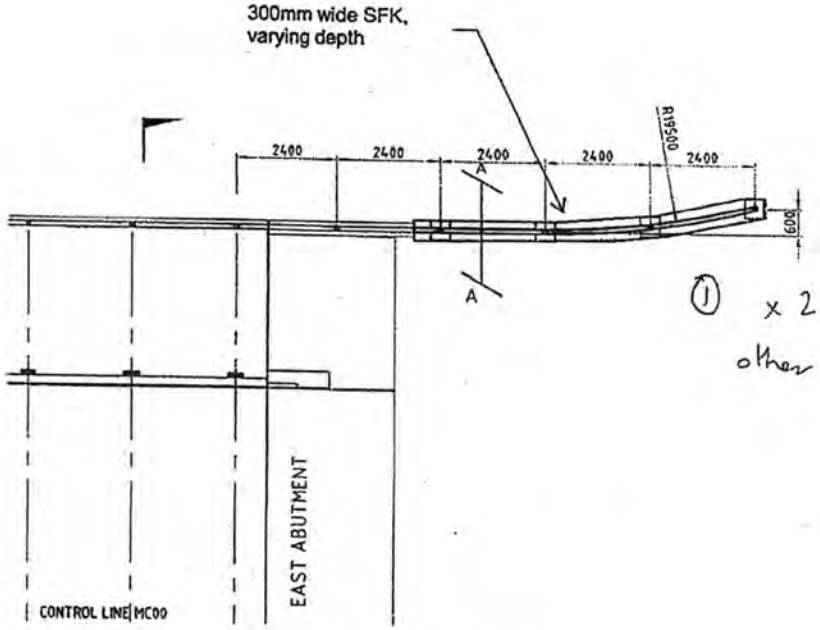


(3)

M110549

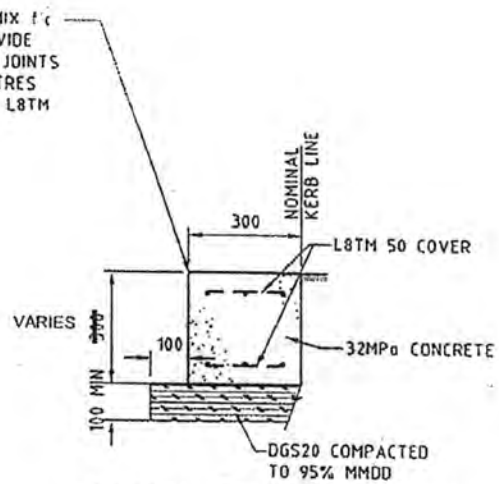


RLedit 28.01.2014 Note: CHs and dimensions approximate



PLAN

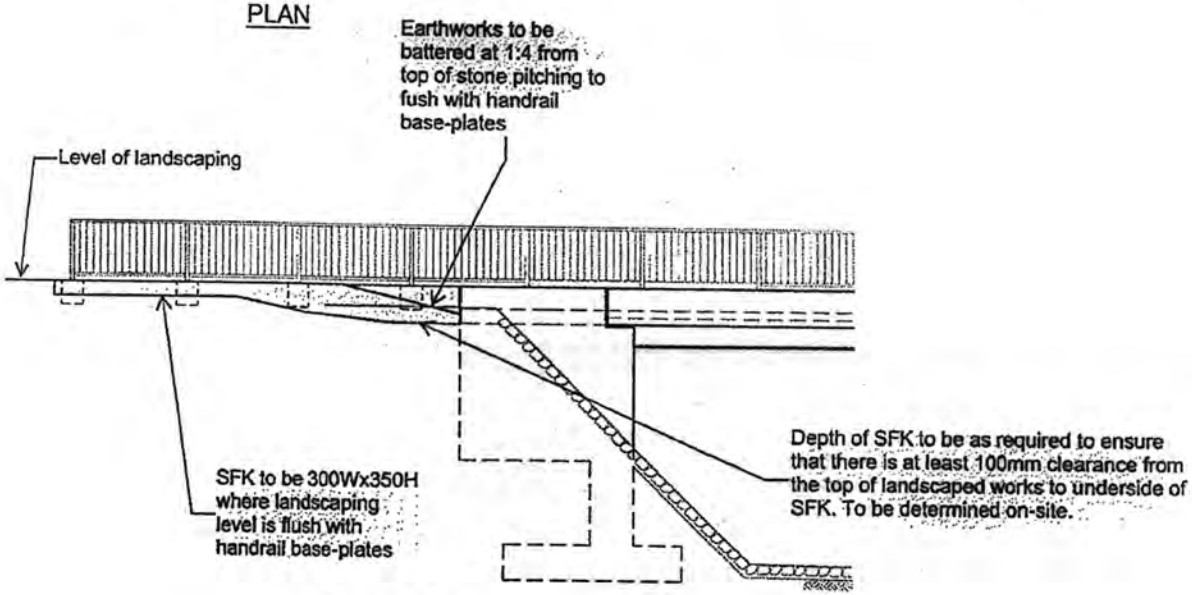
① x 2 to include other side of bridge.



SPECIAL FLUSH KERB (SFK)

SCALE 1:10 @ A1

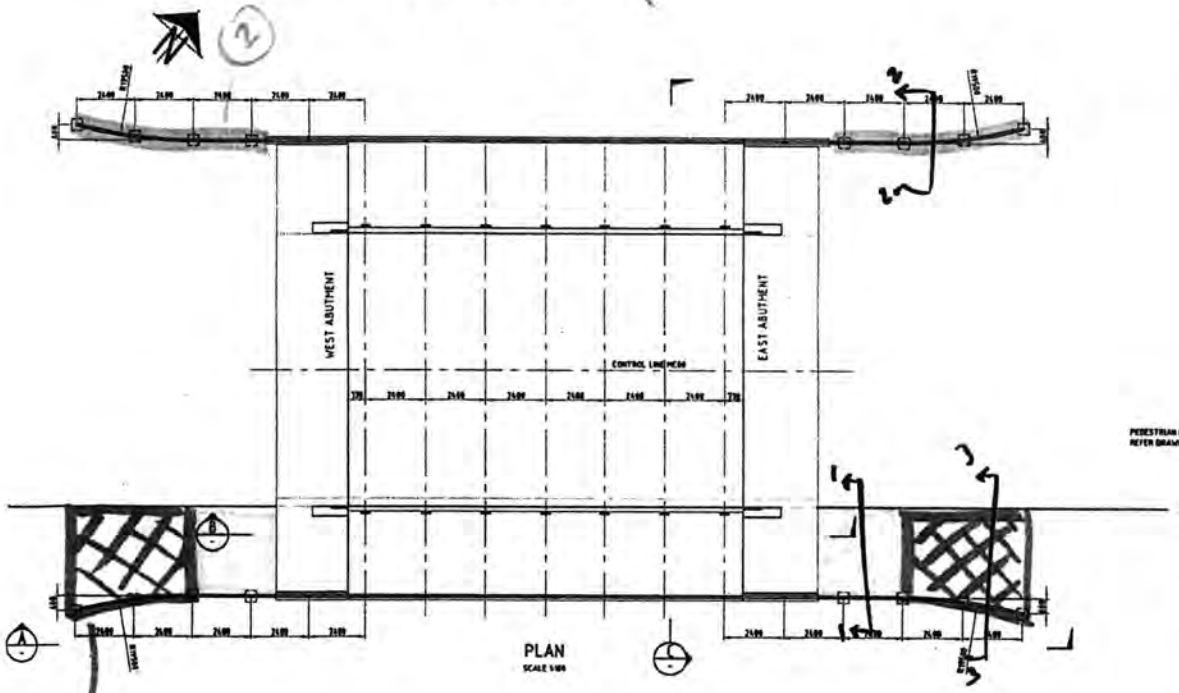
SECTION A-A

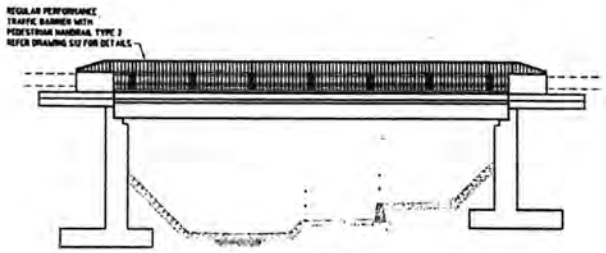


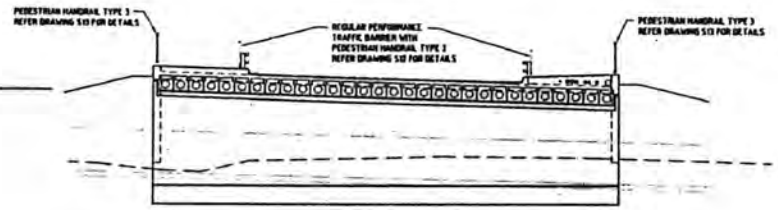
ELEVATION

NOTES:
- Western Abutment arrangement to mirror eastern abutment.
- Not to scale

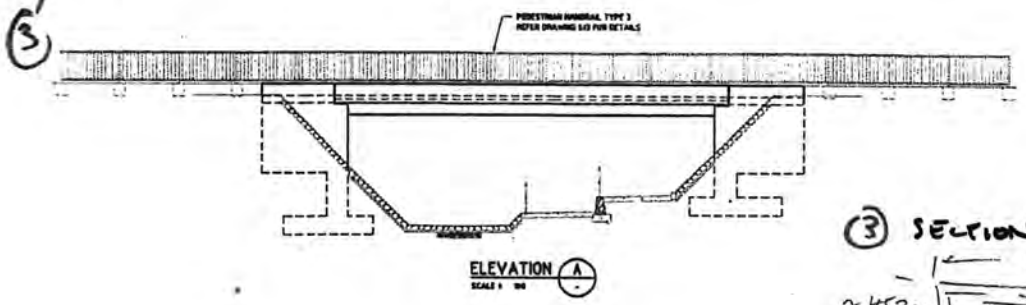
CASEY | TAYLOR



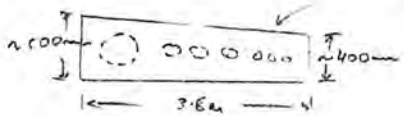




MONCKIEFF

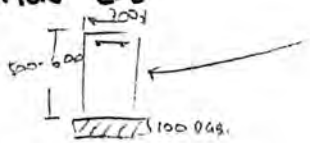


SECTION 1-1



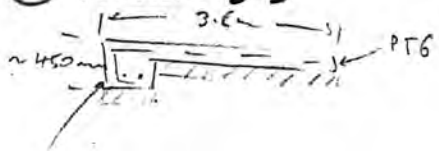
CONCRETE ENCASEMENT W/ PED RAILING FOOTING

(2) SECTION 2-2



FIBRE REINFORCED CONCRETE STRIP FOOTING FOR PED RAILING

(3) SECTION 3-3



THICKENED FOOTING W/ REINFORCEMENT FOR PED RAILING.

NOTES:
1. FOR GENERAL NOTES REFER DRAWING 501

SCALE OF MILLIMETRES 1:100

ISSUED FOR CONSTRUCTION

A	JR	JR	GDM	GDM	24.10.12	
B	JR	JR	GDM	GDM	24.10.12	INCORPORT CHANGES
C	JR	JR	GDM	GDM	9.01.13	ISSUED FOR CONSTRUCTION

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE TO MIRRABEI DRIVE

BROWN Smart Consulting

BARRIER RAILING AND HANDRAIL GENERAL ARRANGEMENT

M110549-S11 B

4

Husband, John

From: @smec.com]
Sent: Friday, 9 May 2014 2:29 PM
To: Husband, John
Cc: Hancock, Patrick; Cargill, James; HPDE
Subject: 2014 05 09 HPD2 - Status of Project Report 9 May 2014
Attachments: Briefing Note - Status of HPD2_Rev3 - 9 May 2014.pdf

John,

Please see revised notes attached with the inclusion of the note re: TTMPs at the road stubs.

Regards,

| Superintendent's Representative
SMEC Australia
T | **M**

From:
Sent: Friday, 9 May 2014 1:01 PM
To: Husband, John (John.Husband@act.gov.au) (John.Husband@act.gov.au)
Cc: Patrick.Hancock@act.gov.au; Cargill, James (James.Cargill@act.gov.au); HPDE
Subject: RE: HPD2 - Status of Project Report 9 May 2014

John,

Please find attached Briefing Notes for HPD2 as per discussions.

Item 8 on page 5 provides details on the value SMEC has certified to date. It is assumed that this would be equivalent to the amount that Hewatt has been paid to date by ACT Government.

Please note the attached has been provided Commercial-In-Confidence.

Regards,

| Superintendent's Representative
SMEC Australia
T | **M**

From:
Sent: Friday, 9 May 2014 9:17 AM
To: Husband, John (John.Husband@act.gov.au) (John.Husband@act.gov.au)
Cc: Patrick.Hancock@act.gov.au; Cargill, James (James.Cargill@act.gov.au); HPDE
Subject: HPD2 - Status of Project Report 9 May 2014

John,

As discussed, please find attached report for HPD2.

Please let me know if you have any questions.

Regards,

| Superintendent's Representative
SMEC Australia
Suite 2, Level 1, 243 Northbourne Avenue, Lyneham, ACT, 2602, Australia
T | **F** | **M**
[redacted] | www.smec.com | LinkedIn

SMEC SNOWY MOUNTAINS ENGINEERING CORPORATION

Local People. Global Experience.

BRIEFING NOTE TO EDD – STATUS OF THE HORSE PARK DRIVE EXTENSION – BURRUMARRA TO MIRRABEI DR

NB: The purpose of this briefing note is to provide information to ACT Government (EDD/SSP) relating to the current status of the Horse Park Drive Extension – Burrumarra Ave to Mirrabei Dr Project.

1. Works Remaining to be Completed

The following Summary provides a general outline of the currently outstanding construction items left to be completed on the Project:

Underground Services

- Installation of RPMs and marker posts for water main valves and hydrants. ACTEW Water are also awaiting the completion of final landscaping around manholes prior to completing the final inspection;
- Connection of water main to existing by ACTEW (already paid by Hewatt).

Pavement

- Completion of trunk cycle path surrounding the existing Telstra Pit and to the entrance to Ngunnawal 2C – approx. 75m to be completed including preparation of subgrade, placement of base material, prime and FGG-R asphalt;
- Pavement profile test pits are yet to have been completed (survey information is available to show extent and conformance of pavement construction).

Concrete Kerbs, Footpaths and Minor Works

- Completion of concrete footpaths beneath bridge – approx. 40% complete of a total area of 850m^2;
- Completion of the concrete footpath extension at Whitrod Ave – approx. 40m^2. Base material has been placed, only concrete pour to be completed.

Road Furniture

- Installation of a single vehicle restriction device;
- Installation of 13 dual post cyclist rest rails. It appears a number of these have been supplied to site and post sleeves have already been installed within traffic islands.

Incidental Works

- Completion of a small section of stone pitching between the floodway edge and the concrete footpath (approx. 25m^2);
- Installation of 300m of PD1 pedestrian fencing along the edge of the concrete paths and along the top of the stone pitched batters (two yet to be completed);
- Placement and compaction of decomposed granite gravel surrounding guardrail – approx. 350m^2 to be completed
- Placement of Jute Mesh/Matting at various places across site (approx. 67% complete of approx. 4,500m^2);
- Demolition and re-construction of the existing Telstra Pit at CH40 (to be completed by Telstra) – ACT Government have raised the potential for these works to be completed by Telstra engaged by ACT Government.

Landscaping

- Minor earthworks are required for the construction of the trunk path at Burrumarra Ave;
- The defects list includes trimming levels for swale inverts and around headwalls and plantation sumps;

- Approx. 5,000m^2 of the Project is yet to be topsoiled and approx. 10,000m^2 is yet to be bitumen straw mulched;
- Planting of 267 native plants (eucalypts) and 96 exotic trees (Malus). No tree planting has taken place, however it is believed that the native trees are ready and available to be brought to site.

Road Signs

- All road signage is yet to be installed. A number of the road sign posts have been installed – however road signage works are effectively 0% complete. Guide signs are believed to have been manufactured ready for installation. Road Sign works include the installation of 64 road signs, including 4 guide signs.
- NB: The road signage works also include installation of treatments for the two road stubs as per approved TTMP.

Pavement Marking

- Pavement marking works are 0% completed.

Traffic Signals

- All traffic signal posts have been installed and wiring is believed to be completed;
- The remaining physical works are associated with the Traffic Controller, including:
  - Completion of concrete apron surrounding controller;
  - Installation of MicroConnects modem supplied by Roads ACT;
  - Connection of the controller unit by Telstra (currently planned for Fri 9 May 2014).
- All handover requirements to TaMS are yet to have been completed.

Streetlighting

- All streetlight poles are complete and wiring to the streetlights and controller box is believed to have been completed;
- SMEC has identified a potential maintenance issue with the location of the streetlights beneath the bridge – these may require relocating;
- All handover requirements to TaMS are yet to have been completed. WAE and asset registers have been sent to TaMS – comments have been received and Hewatt have been requested to revise the WAE drawings.

Bridgeworks

- Anti-graffiti paint yet to be installed on the bridge abutments and sides of the planks/deck;
- Grouting beneath pedestrian handrailing and steel barrier base plates to be completed.

Defects

- SMEC has compiled a list of 65 defects of completed works. Hewatt have been in the initial stages of rectifying these defects. Generally, the majority of these defects are relatively minor in their nature. As the yet to be completed works are carried out, the defects list will be updated accordingly.

2. Status of Project Timing & Program

Hewatt's anticipated completion of physical works date for HPD2, as of Friday 2 May 2014, was Friday 16 May 2014. Therefore, it was Hewatt's estimate that all of the above works could be completed within two weeks. It should be noted that this was most likely based on the consideration that the Telstra Pit and the supply of the Malus trees may be removed from the Project as a Separable Portion.

It is SMEC's prediction that the above works could be finished within three (3) to four (4) weeks if Hewatt had not ceased work on Wednesday 7 May 2014. This would be to complete all civil works and would be dependent on outcomes of the Telstra Pit issue and other potential defects that may be identified during handover inspections.

It should also be noted that the requirements for Practical Completion of the Project as stated within the contract include:

- Completing all works and rectification of defects identified by the Superintendent and relevant accepting Authorities and Departments;
- Acceptances of the works for handover by the relevant Authorities are received by the Superintendent;
- WAE information is approved by the Superintendent.

It is estimated that achieving all of the contractual requirements for Practical Completion may take a considerable amount of time following the completion of all of the physical works.

2.1. Status on the Submission of Quality Records for Completion

Whilst there are countless quality submission requirements as part of the Project, SMEC has provided a summary of the general items that are currently outstanding:

Activity	Status of Submission
Water & Sewer	Revised WAE drawings and comments from ACTEW Water received. Hewatt are yet to submit completed Lot Register and ITPs for submission to ACTEW. Bill of Quantities yet to be received.
Stormwater	WAE drawings and CCTV have been received. Handover requirements yet to be completed. There are a number of quality conformance requirements outstanding.
Conduits	WAE information yet to be received.
Subsoils	WAE drawings have been received.
Earthworks	All conformance requirement Hold Points have been released. Hewatt have hard copies of Lot Register ready for submission.
Pavement	Asphalt: • Ride Quality test completed 8 May 2014, no results received; • A number of Field Sheet results were received 8 May 2014 – SMEC is currently assessing; • WAE for final SMA or FGG pavement outstanding; • Results of core samples still to be received. Various survey/WAE results are yet to be received.
Landscaping	Supply material conformance information received. WAE drawings and asset register to be completed.
Road Signs	Material conformance information for supply received. Practically no work has been carried out on-site.
Pavement Marking	Material conformance information for supply received. Practically no work has been carried out on-site.

Activity	Status of Submission
Traffic Signals	Material conformance information has been received. WAE (and some works) are yet to be completed.
Streetlighting	WAE are to be revised. All other information ready for submission to TaMS.
Bridgeworks	Conformance information received. Hewatt have advised Lot Register and signed ITPs have been completed.

It would take considerable time for SMEC to complete any incomplete quality records from Hewatt. If this is an option that Hewatt and/or the ACT Government wishes to explore, SMEC can review all of Hewatt's records and provide advice on how long it is expected to take to achieve handover.

3. Financial Status

The financial status of the Project has been presented in the tables and sections below:

3.1. Summary of Claimed, Certified and Remaining Amounts

The status of works to have been claimed and certified has been presented in the following table.

The figures presented in items 1, 2 and 3 represent the currently approved Contract Value. This includes the Original Contract Value as per the quantities and amount scheduled in the Bill of Quantities and all currently approved Variation Claims submitted by the Contractor.

Item 7 presents SMEC's estimate of the Final Contract Value. This has been arrived at by considering the Variation Claims that are currently being assessed by SMEC, or are awaiting further information from Hewatt, an estimate of the potential variation claims that would be raised if Hewatt were to complete the works and an estimate of the amount of the scheduled Bill of Quantities that will not be claimed.

This information has been put together to give a guide on the value of the works remaining to be completed. SMEC's rough estimate on the value of the works remaining to be completed is

It should be noted that this number has been arrived at using the amount claimed by Hewatt for April 2014 (as per Item 9) which will be subject to certification by the Superintendent. SMEC are currently assessing the April 2014 Progress Claim and it is expected that not all the amounts or quantities claimed will be certified for payment. It is also noted that the estimated value of works yet to be completed on the Project are utilising Hewatt's rates under the Bill of Quantities.

1 Original Contract Value including Provisional Sums:
2 Approved Variations:
3 Current Approved Contract Value:

4 Variations Submitted that are yet to be approved
5 Potential Future Variations (SMEC Estimate)
6 SMEC Estimate of Bill of Quantity not required
7 SMEC Estimate Final Total Contract Value

8 Total Value Certified by SMEC **(APR '14 CLAIM YET TO BE CERTIFIED):**

9 Payment Claim for April 2014 **(YET TO BE CERTIFIED)**

NB: all amounts are inclusive of GST.

3.2. Summary of major items to be completed

The table below gives a summary of the major items that are yet to have been certified for payment. The table also shows the amount claimed for each item within the April 2014 claim. SMEC has also provided an estimate of the amount remaining to be completed / claimed after April 2014. It is noted that the Superintendent is yet to certify the April 2014 claim and it is expected that not all claimed amounts will be certified.

Horse Park Drive Extension, Burrumarra Avenue to Mirrabei Drive, ACT | Contract no: 2013.20148.110					
Pay Item	Description of Work	Original Scheduled Amount (excl GST)	Scheduled Amount Remaining (excl GST)	Amount Claimed in April 2014 (excl GST)	SMEC's Estimated Amount Remaining after April 2014 (excl GST)
001 PRELIMINARIES					
001P6	Additional Work as Directed by the Superintendent	$			
4.0 FLEXIBLE PAVEMENT CONSTRUCTION					
403P1.6	Cycleways	$			
404P1.1	AMC00	$			
404P1.8	Quick Dry Prime (QDP)	$			
404P2	Supply and Spray Binder - Class 170 Bitumen	$			
404P8	SAMI (S45R) Seal	$			
404P6.2	7mm aggregate (precoated)	$			
404P6.4	14mm aggregate (precoated)	$			
405P7.12	SMA-H 14mm Nominal Size	$			
405P11.3	FGG-R	$			
6.0 CONCRETE KERBS, FOOTPATHS AND MINOR WORKS					
603P2.3	150mm Thick Concrete Paving - Traffic Islands	$			
8.0 INCIDENTAL WORKS					
815P2	Works by Telstra	$			
9.0 LANDSCAPING					
906P1	Grassing	$			
908P1	Bitumen and Straw Mulching	$			
909P1.1	Native Trees 1m Min. spring ring:	$			
909P1.2	Exotic Trees 2.5m Min.	$			
911P1	Consolidation	$			
10.0 ROAD SIGNS					
10	**Road Signs**	$			
11.0 PAVEMENT MARKING					
11	**Pavement Marking**	$			
VARIATIONS					
VO.09	Pricing of Missing Pay Items from the Bill of Quantities	$			
OTHER BILL OF QUANTITY ITEMS AND VARATIONS					
TOTAL ESTIMATED AMOUNT REMAINING (EXCL GST) - LIMITED ACCURACY					

A Microsoft Excel spreadsheet can be provided if further information is required.

4. Current Issues on the Project

The following provides a brief summary on the issues that are currently faced by the Project.

Demolition and reconstruction of the existing Telstra Pit at CH40

Background: there is an existing Telstra Pit located at approx. CH40 within the verge on the Ngunnawal side of Horse Park Dr (near Burrumarra Ave). The cover of the pit currently site approx. 500mm below the design finished surface level (FSL) of the trunk cycle path which is located within 500mm from the side of the pit. The Telstra Pit needs to be raised to match the design FSL. The initial quote provided to Hewatt by Telstra was for a non-ACT IRE Accredited sub-contractor to complete the works.

Status: Telstra are currently identifying a suitable IRE certified contractor to complete the works. Telstra sent out a request for tender which closed on the 22nd of April 2014. As of 8 May 2014, Telstra have not been able to engage a suitable subcontractor. As the pit is part of Telstra's network, they have been explicit that only Telstra are able to complete the works on the pit.

SMEC's Recommendation: given the current situation it would be SMEC's recommendation that ACT Government engage Telstra to complete the works (rather than Hewatt). This will allow planning of the works to continue whilst Hewatt have ceased work. If this is an option, ACT Government would need to liaise with Hewatt to allow Telstra to gain access to the site, as Hewatt are the Principal Contractors.

Delays in the supply of the Malus Spectabilis for planting

Background: the landscape drawings call up Malus Spectabilis (Crab Apples) to be installed on the corners of each intersection. The landscape designer has advised that the detailing of these trees was required under the Horse Park Drive landscape "master plan". 2014.

Status: due to the timing of the completion of the project, the Malus Spectabilis trees are not able to be supplied bare-rooted to site. The supplier has informed Hewatt that the earliest these will be able to be delivered to site is at the end of June.

SMEC's Recommendation: SMEC recommends that the planting and consolidation of the Malus Spectabilis be extracted as a Separable Portion. The planting of the trees can be completed when the road has been opened to traffic.

The current date for Practical Completion

Background: the current date for Practical Completion was on 28 April 2014. The dates for Practical Completion were revised by mutual agreement between ACT Government and Hewatt as part of a proposal by Hewatt to coincide the completion of all separable portions within Horse Park Drive "Stage 2" and the remaining separable portions of Horse Park Drive "Stage 1".

This proposal resulted in the date for Practical Completion of Separable Portion 3 for HPD2 (the last separable portion for the two Horse Park Drive Extension Projects) being brought forward by approx. 3 months. It was the intention that traffic would be able to use the entire length of Horse Park Dr from Amaroo to Casey three months earlier than expected. The proposal also came with a cost saving to the ACT Government of approx.)

Status: given the works remaining to be completed on the Project it is expected that Practical Completion will not be achieved until 1 – 2 months following the current date for Practical Completion. It has been noted by Hewatt, the ACT Government has benefited from the proposal both financially and in delivering the entire Horse Park Drive Extension prior to the original date for PC for SP3.

Hewatt also noted that there have been a number of delays to the Project. Some of these delays (eg. Telstra Pit) may entitle Hewatt to an Extension of Time under Clause 35.5 (b) of AS 2124-1992.

SMEC's Recommendation: it is SMEC's recommendation that the Superintendent grant an Extension of Time to extend the date for PC to 6 June 2014 for all separable portions under Clause 35.5.

The EOT will account for delays to the Contractor's program caused by events covered within Clause 35.5 (b) of AS 2124-1992. The most significant of these delays has been the Telstra Pit which has affected critical path activities, including the trunk path. The EOT will be granted on the condition that Hewatt will not be entitled to costs associated with the delay as they did not take all reasonable steps to preclude the occurrence of the cause or minimise the consequences of the delay.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd

E: @smec.com Ph:

Husband, John

From: @brownconsulting.com.au]
Sent: Thursday, 8 May 2014 3:48 PM
To: Husband, John; Hancock, Patrick
Cc: McGrath, Michael
Subject: RE: Horse Park drive Stage 1 - Assessment of Outstanding Documentation and work to complete Project

John

Please see a summary below of where things are at on Horse Park Drive #1:

Authorities

- WAE for watermain handed to Actew but at present connection has not been undertaken. I understand that the works have been booked but unsure of date
- WAE for sewer handed to Actew but at present final acceptance not received (need plug removed)
- WAE for civil works – to be handed as one package, still waiting for SP4 documents
- Box culverts haven't been handed over due water seepages
- Handover of landscaping (final) for SP#3
- SP5 consolidation of landscaping hasn't been achieved

Works

- Minor landscaping works
- Linemarking
- Signage
- Minor subsoil works around box culvert to aid in water entering culvert
- One floodlight missing from Moncrieff
- We raised concerns of finish of SMA in Jacka #2 with Hewatt and Hewatt in turn raised with Downer but no response at present
- Final defects walk for SP4 hasn't been undertaken. List done 2 months ago, so no major items should be present, likely just cracking of kerbs, etc.
- TTM for treatment of stubs (note no variation submitted at present)

QA Docs

- QA folder for SP4 hasn't been received
- QA documents for the last lot of SMA hasn't been received
- Asset forms for SP4 haven't been received

Financial

- Paid to date:
- April Claim received on 6/5/14: **(note not assessed at present, this a rough guide and assumes full SMA payment will be approved)**
- BOQ amount remaining :
  - Pay Item 1P7 "WAE " =
  - Pay Item 204P8 "Removal of Watermain" =
  - Pay Item 304P11.1 "Sewer Connection" =
  - Pay Item 308P5.1 "Water Connection" =
  - Pay Item 308P5.2 "Works by Contractor" =
  - Pay Item 405P7.12 "SMA" =

- Pay Item 803P1 " Stone Pitching" =
- Pay Item 911P1 "Consolidation" =
- Signage =
- Line Marking =
- Pay Item 1406P1 "Co-ordination for Streetlights" =
- **Subtotal =**

- Variations remaining:
  - Jacka Landscaping =
  - Items not priced yet =
  - **Subtotal = !**
- **TOTAL REMAINING TO BE PAID =**

Estimated final contract sum = This figure also does not include the potential change in grated pits work up near Jacka #2, which with a rough estimate maybe about or so.

All above figures are inclusive of GST.

Please let me know if you need anything further

Regards

| Senior Engineer - Road & Traffic
Brown Consulting (ACT) Pty Ltd

BROWN

Smart Consulting

GPO Box 261, Canberra, ACT , 2601
Level 6,121 Marcus Clarke Street, Canberra City, ACT, 2601

t. 02 6211 7100 | f. 02 6211 7199
e. @brownconsulting.com.au
www.brownconsulting.com.au

From: Husband, John [mailto:John.Husband@act.gov.au]
Sent: Thursday, 8 May 2014 1:51 PM
To:
Cc: McGrath, Michael
Subject: Horse Park drive Stage 1 - Assessment of Outstanding Documentation and work to complete Project

Hi

Can you please provide an Assessment on the following :

Outstanding Quality documentation
Approvals by any third parties, TAMS, Actew etc
Amount of work to complete project

The assessment needs to include time and cost implications.

Can you provide this information by tomorrow

Thanks

John Husband | Senior Project Manager
Phone 02 6205 2920| Fax 02 6207 6621| Mobile
Infrastructure and Capital Works| Economic Development Directorate | **ACT Government**
Level 6, TransACT House, 470 Northbourne Ave, Dickson ACT 2602 | GPO Box 158 Canberra ACT 2601 www.edd.act.gov.au

Husband, John

From: Husband, John
Sent: Thursday, 8 May 2014 2:18 PM
To:
Subject: FW: Cotter / Kirkpatrick Outstanding Monies / Transfer

Hi Scott

The transfer has been approved. Could you please process as part of the latest claim for Horse Park Drive Stage 1

Thanks

John Husband | Senior Project Manager
Phone 02 6205 2920| Fax 02 6207 6621| Mobile
Infrastructure and Capital Works| Economic Development Directorate | **ACT Government**
Level 6, TransACT House, 470 Northbourne Ave, Dickson ACT 2602 | GPO Box 158 Canberra ACT 2601 www.edd.act.gov.au

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om: [mailto: @hewatt.com.au]
Sent: Tuesday, 6 May 2014 1:28 PM
To: @blackmtn.com.au; Dickson, Peter; Husband, John; Hancock, Patrick; (@brownconsulting.com.au)
Subject: Cotter / Kirkpatrick Outstanding Monies / Transfer

All,

With financial wrap up of the Cotter Road project, there is a debt due from Hewatt to the Principal to the value of

Hewatt have a Bank Guarantee in place for this project. In order to have the outstanding monies paid to the ACT Government, there a number of options

- Deduct monies from the Bank Guarantee
- ACT government issue Hewatt a Tax Invoice for the amount
- The amount be deducted from another project which Hewatt are undertaking on behalf of the ACT Government.

Deducting money from the BG is not a preferred option due to the slow processes required, and from the governments perspective, sending an invoice I don't expect would be preferred.

Hewatts preferred solution, if possible, is to have the money deducted as a negative variation to the Horse Park Drive 1 project with the variation clearly identified as being for the outstanding monies. I am not sure how practical this is for either the government or Browns.

The month of April would be ideal for this as the Progress Claim value will be of sufficient value due to SMA works to cover this deduction.

Please advise at your earliest convenience if this is acceptable such that the amount can be included into the April HPD claim currently being prepared for Browns.

Please contact me if there are any queries or concerns.

Thanks

Regards

Engineering Manager

PO Box 94 HALL ACT 2618
T | F | M
r@hewatt.com.au | www.hewatt.com.au

Communication Honesty Accountability Respect Teamwork

Husband, John

From: @brownconsulting.com.au]
Sent: Wednesday, 7 May 2014 9:59 AM
To: Husband, John
Cc: Hancock, Patrick; McGrath, Michael
Subject: RE: Horse Park Drive Extension Stages 1 and 2 - Hewatt Advice re closure of Site

John

Having spoken to this morning the site is shut today for the reason that the negotiations between Hewatt and Fulton Hogan have stalled thus Hewatt did not want to undertake an illegal activity of trading when possibly insolvent. At present it is a day by day process with the hope that an agreement can be reached as soon as possible.

also mentioned there was a radio report indicating the company was in liquidation now that is not the case

Apparently all jobs excluding Majura are fine it is just the issues with Majura have a massive impact on the company.

Regards

| Senior Engineer - Road & Traffic
Brown Consulting (ACT) Pty Ltd

BROWN

Smart Consulting

t. 02 6211 7100 | f. 02 6211 7199
GPO Box 261, Canberra, ACT , 2601 e.: @brownconsulting.com.au
Level 6,121 Marcus Clarke Street, Canberra City, ACT, 2601
www.brownconsulting.com.au

From: Husband, John [mailto:John.Husband@act.gov.au]
Sent: Wednesday, 7 May 2014 9:31 AM
To:
Cc: Hancock, Patrick; McGrath, Michael
Subject: Horse Park Drive Extension Stages 1 and 2 - Hewatt Advice re closure of Site

and :

Could you please request from Hewatt in writing, the reasons for closing the two sites today as a matter of urgency.

Regards

John Husband | Senior Project Manager
Phone 02 6205 2920 | Fax 02 6207 6621 | Mobile
Infrastructure and Capital Works | Economic Development Directorate | **ACT Government**
Level 6, TransACT House, 470 Northbourne Ave, Dickson ACT 2602 | GPO Box 158 Canberra ACT 2601 www.edd.act.gov.au

8

QUESTION TIME BRIEF
Sitting period beginning 6 May 2014

SUBJECT: HORSE PARK DRIVE EXTENSION STAGE 1 AND STAGE 2

ISSUE: Status of works completed by Hewatt Pty Ltd

KEY MESSAGES:

- Stage 1 of Horse Park Drive Extension from Katherine Avenue to the Moncrieff Group Centre was awarded to Hewatt in July 2012. The project is 95 per cent complete with only minor works remaining.
- Stage 2 of the Horse Park Drive Extension from Burramurra Avenue to Mirrabei Drive was awarded to Hewatt in February 2013. The project is 95 per cent complete with only minor works works remaining.
- The two projects are currently programmed for completion by mid May 2014.
- Shared Services Procurement (SSP) is managing the projects on behalf of Economic Development Directorate. SSP have advised that all payments due and payable to Hewatt have been made in accordance with the terms and conditions of the contracts.
- The payment claims for works in April 2014 are currently being assessed by the Superintendent.

BACKGROUND INFORMATION:

- Both Horse Park Drive projects have been satisfactorily carried out by Hewatt.
- There have been no contractual disputes raised with the Territory to date on the two projects by either Hewatt or their subcontractors.
- Horse Park Drive Stage 1 was awarded for $16.9 million of which $14.98 million has been paid to date. A final claim for final works to be completed is anticipated to be less than $50,000.
- Horse Park Drive Stage 2 was awarded for $9.1 million of which $7.1 million has been paid to date. The claim for payment for works in April 2014 of $1.2 million has been received from Hewatt.
- It is estimated there may be up to 12 staff and 20 subcontractors engaged by Hewatt covering both sites. Staff numbers are an estimate and need to be confirmed with Hewatt.

Negotiations are continuing with Administrator to have the remaining works completed by end June 2014

Glenn Lacey, Director, Infrastructure and Capital Works
Phone Number: 62075507

Updated: 7 May 2014

Caveat Brief

Ref:

To	Minister for Economic Development
From	Director-General, Economic Development Directorate
Subject	Horse Park Drive Extension Stage 1 and Stage 2 – Hewatt Closure of Sites 6 May 2014

- The Superintendent (SMEC) on Stage 2 of this project was advised this morning by , the Hewatt Senior Project Manager of the two Horse Park Drive Projects, that no work was to be carried out today on the sites. This stage of the project is over 85% complete with line-marking and some landscaping to be completed. Antony advised that he was not sure when works would re-commence

- Written confirmation was requested by EDD immediately on the reasons for this closure.

- Stage 1 of the Horse Park Extension from Katherine Ave to the Moncrieff Group Centre is over 90% complete.

- These two projects are due for completion mid May 2014.

- SSP have advised that all payment claims from Hewatt have been processed as per the contract conditions.

- A draft payment claim for April 2014 has been submitted by Hewatt for assessment by the Superintendent. Amount $1.3M ex GST

David Dawes

Husband, John

From: @hewatt.com.au]
Sent: Tuesday, 6 May 2014 1:28 PM
To: @blackmtn.com.au; Dickson, Peter; Husband, John; Hancock, Patrick; @brownconsulting.com.au)
Subject: Cotter / Kirkpatrick Outstanding Monies / Transfer

All,

With financial wrap up of the Cotter Road project, there is a debt due from Hewatt to the Principal to the value of

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Hewatts preferred solution, if possible, is to have the money deducted as a negative variation to the Horse Park Drive 1 project with the variation clearly identified as being for the outstanding monies. I am not sure how practical this is for either the government or Browns.

The month of April would be ideal for this as the Progress Claim value will be of sufficient value due to SMA works to cover this deduction.

Please advise at your earliest convenience if this is acceptable such that the amount can be included into the April HPD claim currently being prepared for Browns.

Please contact me if there are any queries or concerns.

Thanks

Regards

Engineering Manager

HEWATT

PO Box 94 HALL ACT 2618
T F | M
@hewatt.com.au | www.hewatt.com.au

Communication Honesty Accountability Respect Teamwork

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

WEEKLY REPORT FOR WEEK ENDING: 3 May 2014

1. **WHS**
   a. Site inductions continuing, 219 personnel to date

2. **Traffic Management**
   a. No changes – construction vehicles using carriageway (with SMA) for access throughout the site.

3. **Design**
   a. *Clarification on TCDs and termination of path adjacent to Whitrod Ave (RFI121A)* – response has been issued to Hewatt. The response includes the addition of a concrete footpath at Whitrod Ave.
   b. *Interface between trunk path and pram crossings (RFI123A)* – Hewatt have raised a query regarding how the trunk paths are to be tied in to the pram crossings at Road 04 and Mirrabei Dr Intersections. SMEC will reply to this RFI today.
   c. *Temporary TCD for road stubs* – RD Gossip has received approval of the TTMP from Roads ACT. SMEC will forward these onto Hewatt.

4. **Construction**
   a. Placement of SMA pavement completed
   b. Stone pitching at the northern side of the floodway and under the bridge completed;
   c. Preparation of base for the equestrian path completed and placement of concrete completed to CH70;
   d. Preparation of base for the shared path under the bridge continuing;
   e. Placement of decomposed granite under the guardrail completed;
   f. FGG asphalt placed on the shared path from HPD1 to CH90. The last section to a tie in with Ngunnawal will be completed when the Telstra pit issue is resolved;
   g. Concrete infill in traffic island completed;
   h. Pram ramps for the interface between carriage way and shared path completed;
   i. Footpath base and formwork for the footpath between Burrumarra intersection pram ramp and Casey footpath completed;
   j. Landscaping works across site continued including removal of sed and erosion controls and the spreading of topsoil.

5. **Meetings**
   a. Fortnightly site meeting held on Friday 2nd May 2014. Minutes will be sent through shortly.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Telstra are still looking for an industry specialist with ACT IRE Certification. Tenders closed 22 April 2014. Hewatt are in frequent contact with Telstra to get any updates on the situation. John Husband was going to try to call r to see if the issue can be progressed;
   b. *Excess material potentially requiring disposal* – spreading of stockpile material has commenced and it appears no disposal of spoil of site will be necessary. Final shaping works are being prepared;

c. *Stone-pitching finishes (ISS022)* – Hewatt, SMEC, EDD and SSP will meet on Thursday 8 May 2014 to try to achieve an outcome.

7. Payments

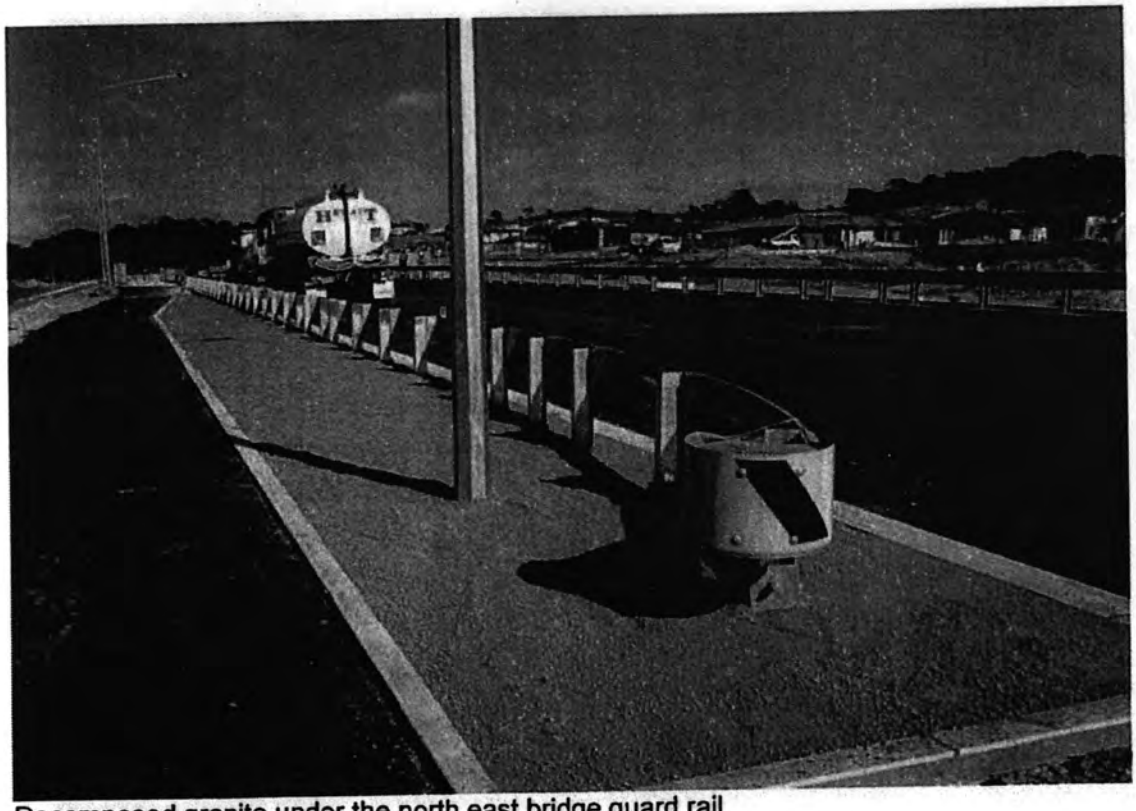
a. Progress Claim No. 13 – April 2014 to be submitted on Wed 7 May 2014;
b. SMEC has notified Hewatt that payment will only be certified when quality documentation is received.

8. Activities Planned for the Upcoming Fortnight

a. Commence line marking and installation of signs;
b. Complete placement of concrete in the equestrian and shared paths under the bridge;
c. Landscape activities to continue across the site – topsoiling, seeding, straw mulching and bitumen spraying;
d. Tree planting to commence
e. Construct concrete footpath from Burrumarra Avenue intersection pram crossing to Casey footpath ;
f. Complete interface between the shared path and carriageway pram crossings ;
g. Commence rectification of defects – cracks in K&G, etc.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos



Decomposed granite under the north east bridge guard rail

Placement FFG asphalt on the shared path

Placement of concrete for north east bridge abutment mowing strip

Equestrian and shared paths

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

MEETING: SITE MEETING - MINUTES

LOCATION: Project Site Office, Horse Park Drive, Jacka
DATE: Friday, 2 May 2014
TIME: 10.30am
ATTENDEES:

Economic Development Directorate: *Michael McGrath (MM)*
John Husband (JH)
Michael Sergi (MS)

Shared Services Procurement: *Patrick Hancock (PH)*
James Cargill (JC)

Brown:

SMEC:

Hewatt:

No.	Item Description	Action
01.	WHS • Hewatt Safety Manager advised that WHS "Attachment 5" Checklist for EDD will be completed today. will forward when received from Hewatt. • The report from the ACT Govt. WHS Audit held on 17 March 2014 by Kaizen has not yet been received. JC will chase this up. • noted that last meeting it was asked whether the WHS Guidelines used by EDD (from which Attachment 5 comes from) would be made available to consultants and contractors. JH noted that this would be beneficial to everyone and will investigate.	Hewatt/ JC JH
02.	Review Previous Minutes / Matters Arising • JH will continue to keep EDD communications in the loop regarding a potential date for an opening event. It was noted that it is unclear whether the event will be held when the road is open to traffic or prior.	JH
03.	Design • RFIs: 123 submitted; 1 open • All queries and issues relating to the changes to SMA design and specifications have been resolved and the SMA works have been completed. Hewatt is happy with the response to queries from SMEC/EDD. • will raise an RFI regarding the use of temporary kerbing at Burrumarra Ave tie-in. It was noted that temporary kerbing commonly involves pins, which may be troublesome with only a thin layer of SMA. • SMEC raised RFI121A to Brown asking for clarification on a number of items. Brown's response included confirmation to extend the concrete path coming from Casey beside Whitrod Ave and the installation of an additional pram ramp at Mirrabei Dr that had been omitted from the design. • & confirmed details of the chase required at Burrumarra	

	Ave shortly after the previous site meeting. noted that it seems to work/ride well. • There is only one open RFI regarding a request for details at the interface between the trunk path and pram crossings. noted that a neater solution may be for the use of short sections of concrete path. will formally respond to the RFI.	
04.	Site Compound • Hewatt will be looking to start removing the site offices on Monday 12 May 2014.	
05.	Program • gave a brief update on the state of the Project: o SMA complete o Cyclepath – 95% complete o Most of the work left is beneath the bridge. • noted that all physical works are planned to be completed by end of next week (Fri 9 May 2014). There are currently a few issues holding up construction (refer Section 12), including: o Completion of reconstruction of Telstra Pit o The supply of the exotic Malus trees (Crab apple trees) – can't supply until July 2014. • opened discussion about the current date for PC and the likely completion date. asked if ACT Govt. could advise on the status of whether liquidated damages would be enforced. noted that HPD2 will be opened prior to the original PC date for SP3 and generally the contract has been carried out in a positive manner with only 1 delay claim being submitted at the start of the project. said there are a few EOTs that could be submitted, however raising and responding to these may take up time better spent completing the project ASAP. JH noted the comments and noted that had sent an email raising a number of similar points. JH will discuss internally and suggested that this topic could potentially be discussed at the stone pitching meeting on Tuesday (6/5/14) (**Post meeting note – date of meeting has now been changed**). **Current PC Dates** SP1 & SP3 – Civil, Burrumarra Ave – HPD1 – 28 Apr 2014 SP2 & SP4 – Landscape, Burrumarra Ave – HPD1 – 27 Oct 2014	JH/ SMEC/SSP/ Hewatt
06.	Environmental • An EPA representative did visit site, however it was to have a look at Taylor rather than inspecting HPD2. • noted that some of the environmental controls are being removed as areas of landscaping start to establish. • Rain fell on the night of 29/4/14 and left areas beneath the bridge saturated.	
07.	Industrial Relations • Refer Section 12	
08.	Traffic • Traffic lights at Mirrabei Dr: outlined status of the traffic light installation: o Signals have been installed o Traffic light controller has been installed and is awaiting Telstra to verify connection and for the modem to be installed. • The supply and payment for the modem has been a point of	

	discussion between Roads, SMEC, Hewatt and Ecowise. noted that Ecowise have not had to pay for it in the past and said it will form a Variation. JH noted that for) that this can just be paid as a Variation and there is no need for it to delay the Project. to order modem.	
09.	Community • JH has responded to a couple of queries from the public about likely completion dates for the Project. One of the enquiries was through Twitter. • met with the foreman from Canberra Contractors on Ngunnawal 2C to talk about the installation of one of the HPD2 guide signs on the N2C sound mound.	
10.	Staffing • (Foreman) has moved onto Majura Parkway. There are currently two leading hands full time on the Project and (Foreman) part time on the project until completion.	
11.	Utilities • Streetlighting: lights beneath the bridge were delivered yesterday and are to be installed early next week – this will complete physical works. Comments received back from SMEC re: WAE drawings. Hewatt will revise and resubmit. HPD1's streetlighting WAE have been submitted to TaMS. • Water and Sewer – final inspection has to wait until all works are complete (ie. RPMs in place and landscape levels final). After final inspection and acceptance of all the quality information and WAE, the connection to existing for water can be completed. This is one of the requirements for Practical Completion under the Contract. • Gas – gas is live across site and a final inspection will be completed by ZNX/Jemena once all works are complete. • Telstra – Telstra will send Hewatt WAE for works completed. • NBN Co. – WAE need to be submitted to NBN Co. and then a final completion inspection will be held. will resend details of WAE requirements. • ActewAGL – all power has been switched on and is live to mini-pillars across site.	
12.	Issues **Telstra Pit** • Telstra are still looking for an ACT IRE certified industry specialist to reconstruct the existing Telstra pit at CH40. This issue has been with Telstra for some time. noted that this will likely be the last thing to be completed (or there is the potential for it to be taken out as a separable portion). asked whether EDD or SSP have had any luck by speaking with Telstra in the past. JH and JC noted that they don't have much sway with Telstra. & noted that and have been regularly contact to ask about the status of the issue. JH will contact r to see if he can have an influence. • JC will follow up on how long gaining ACT IRE certification is likely to take. • said Hewatt will continue to complete the cyclepath and landscaping on the other side of the Telstra pit, allowing room for future access and excavation of the pit. All works apart from FGG will be completed (a decent run of FGG is desirable for quality).	JH JC Hewatt

	Supply of Malus • noted that the landscape supplier does not expect the Malus (Crab Apple/Exotic) trees to be available until the end of June 2014. noted that alternatives to the Crab Apples would probably not be possible as Crab Apples at the corner of intersections is part of the Horse Park Dr "Masterplan" – however Hewatt should raise this question as an RFI. • also noted that the BoQ requires the trees to be 2.5m tall – which is very mature, given they only grow to 3-4 metres. noted this could also be queried in an RFI. • noted that the Malus could form a separable portion – JH noted that in a couple of years' time, no-one would know if the trees were planted a couple of weeks/months late. **Stone-pitching** • There will be a meeting on Tuesday 6 May 2014 (**Post-meeting note – day has changed**) between EDD/SMEC/SSP/Hewatt with the intention of coming to a resolution on the stone pitching issue. **Handover/Completion** • requested that a final inspection could start to be organised by SMEC/SSP. asked who would attend a final walk-through – noting that a clear direction on defects, etc. is required so issues can be closed and are not continually progressively raised. will send a note through to PH requesting a handover inspection be organised. • to send Hewatt a revised Defects List.	Hewatt
13.	EOT • EOT Status (17 submitted): 65.5 Days Claimed; 49.5 Days Granted • Dates for PC and likely completion dates were discussed with program – refer Section 05.	
14.	Variations • **Status** (including BoQ over/underruns) (excl GST): Submitted; Endorsed; Certified for payment (as at end of Mar '14) • Confirmation on SSP/EDD's agreement on was received and an official Variation letter sent to Hewatt by SMEC. • will revise VO.31 to encapsulate all works associated with the electrical supply to the SLC and TLC.	
15.	Progress Claims • Hewatt's Progress Claim for April 2014 will be submitted on Wednesday 7 May 2014. • JH noted that all quality documents demonstrating conformance of the SMA must be submitted prior to payment (as per the Contract). noted that he had received an email outlining this. indicated that in terms of SMA, he will be claiming 100% of the SMA payment within the claim. All the quality documentation may not be available at the time of submission, however they should be ready at the time of certification. Therefore it will be left to SMEC to certify quantities as required given the available quality submissions.	
16.	Quality Assurance • HPs: 349 raised; 17 open • NCRs: 32 raised	

	• The bump test will be carried out on Thursday 8 May 2014. Results should be available prior to certification of the April 2014 progress claim. asked for details on how turning lanes, etc. are dealt with in the ride quality test? would investigate, but suggested it be raised as an RFI. MS added that the crew doing the testing would know what to do. • It was noted that a corrective course for the bridge deck was not required following survey after the SAMI seal. There were a number of points where the course thickness was a bit thick, but the alternative was to have a very thin corrective layer. noted importance of getting levels for deck pour correct. • Hold Point for the SMA mix design, etc. has been released. • There are a number of Hold Points awaiting information to be submitted – where works have proceeded this information is generally just for formalities (eg. signed certificate of conformity, etc.). The Open Hold Point Register was included with the meeting's papers.	
17.	Working hours and days • Nil	
18.	Construction Matters • RD Gossip are seeking Roads ACT approval for the TTMPs for road stubs – it is expected this will be gained by early next week. SMEC will then issue an instruction to Hewatt for the completion of the works (barriers, etc. would become the property of the ACT Government upon installation). • will complete a pavement profile report for the ends of the road stubs – including photos and measurements. noted that construction of each layer has been extended past the official limit of works (eg. SMA layer tapers to zero past the limit of works) – the test pits should be taken at an appropriate location (which will be indicative of the constructed pavement within the LoW's). noted that survey conformance would be included in the report. • will be conducting a walk-through with Downer on Monday 5 May to discuss a number of items that was not happy with – around kerbs and joints at intersections. asked for SMEC's comments on the SMA by midday Monday. • MS asked if an NCR had been raised for SMA works that were completed on the same day Glenn Lacey and Ben McHugh were on-site. No NCR had been raised – it was assumed it was in relation to placing SMA on a rainy day. outlined that no rain had fallen in the morning, SMA works were carried out in the stubs as rain was expected, very light rain fell as Downer were completing their last run in Mirrabei Dr (suggested the SMA would have received more moisture from the rollers than from any rain). The meeting with GL and BM was in the afternoon after rain had fallen and the surface was wet when they were on-site. GL raised the issue. No NCR has been submitted. **Post-meeting note: has requested Hewatt to raise an NCR for placing SMA when rain was "imminent".**	SMEC SMEC SMEC/
19.	Subcontractors • All major subcontractors have all been engaged; • Kuna have been engaged to carry out some minor works on the Project, have let Hewatt down recently.	

20.	Next Meeting(s) • Friday, 16 May 2014 – Site Meeting (if required) • JH noted this may be a good date for the walk through/handover inspection.	
21.	Other?	

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

WEEKLY REPORT FOR WEEK ENDING: 26 April 2014

1. **WHS**
   a. Site inductions continuing.

2. **Traffic Management**
   a. No changes – construction vehicles using carriageway (with SMA) for access throughout the site.

3. **Design**
   a. *Clarification on TCDs and termination of path adjacent to Whitrod Ave (RFI121A)* – Brown responded to RFI121A. This response included the requirement for an additional length of path along Whitrod Ave and an additional pram ramp crossing.
   b. *Temporary TCD for road stubs* – RD Gossip has prepared a design for the proposed TCDs for the road stubs. EDD and SMEC have raised comments and responded to Gossip. Gossip to seek approval from Roads ACT.

4. **Construction**
   The site was generally shut down for the Easter weekend to the end of the ANZAC Day weekend. There were some construction activities by subcontractors 22nd to 24th April. This included:
   a. Stone-pitching continued beneath the bridge and for the northern bridge embankment;
   b. Preparation for concrete pours for footpaths adjacent to the floodway continued;
   c. Concrete poured for mowing strips surrounding red granite beneath guardrail;
   d. Landscaping works across site continued including mowing of grass and spreading of topsoil.

5. **Meetings**
   a. There were no meetings. The next fortnightly site meeting is scheduled for 2nd May 2014.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Telstra's has requested for tender for the works (with the requirement that the subcontractor is to be ACT IRE Certified). Tenders closed 22 April 2014. Hewatt are hoping to hear from Telstra early this week (w/c 28/4/14);
   b. *Excess material potentially requiring disposal* – spreading of stockpile material has commenced and it appears no disposal of spoil of site will be necessary;
   c. *Stone-pitching finishes (ISS022)* – SMEC has issued an instruction to Hewatt to repair the area of stone pitching on 16 April 2014. SMEC has invited Hewatt to dispute this direction under Clause 46.2 – AS2124. Hewatt have 56 days to dispute the direction by issuing a Notice of Dispute. Hewatt will respond this week (w/c 28/4/14).

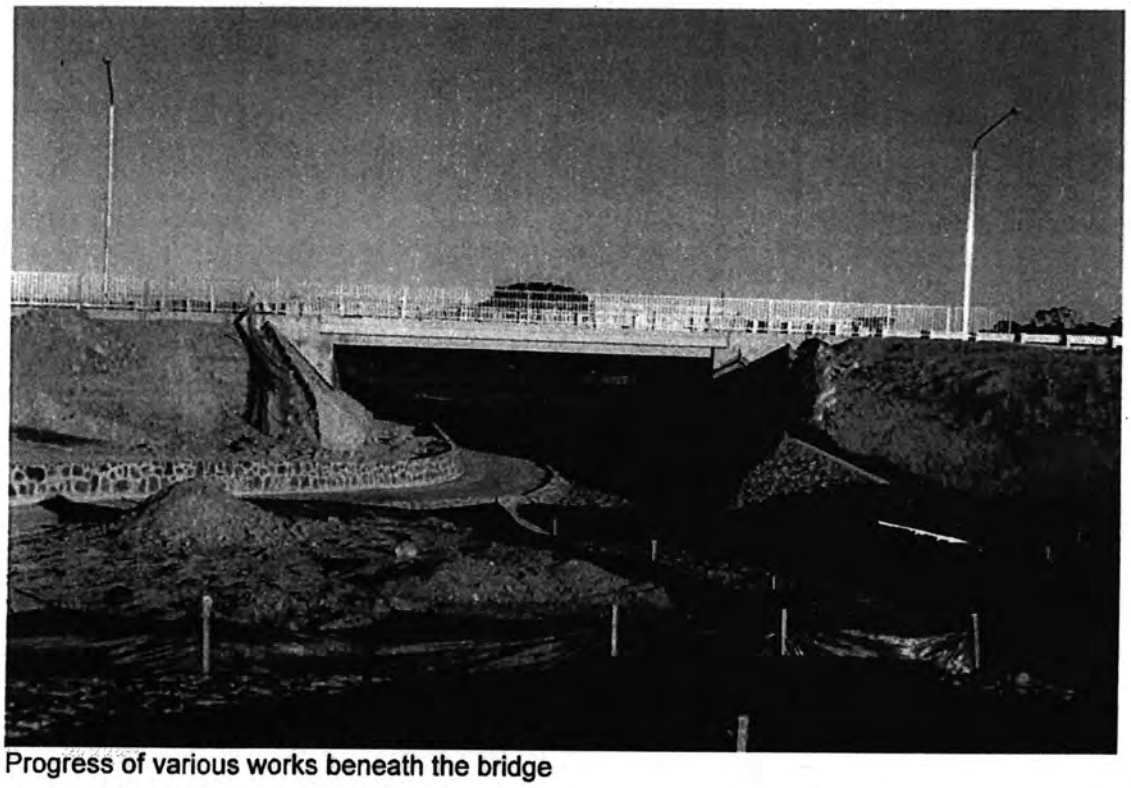
7. **Payments**
   a. Variations and quantities for Payment Claim No. 13 (April 2014) currently being reviewed and assessed by Hewatt and SMEC;
   b. SMEC will be gathering Hewatt's quality records for works being claimed in the April progress payment.

8. Activities Planned for the Upcoming Fortnight

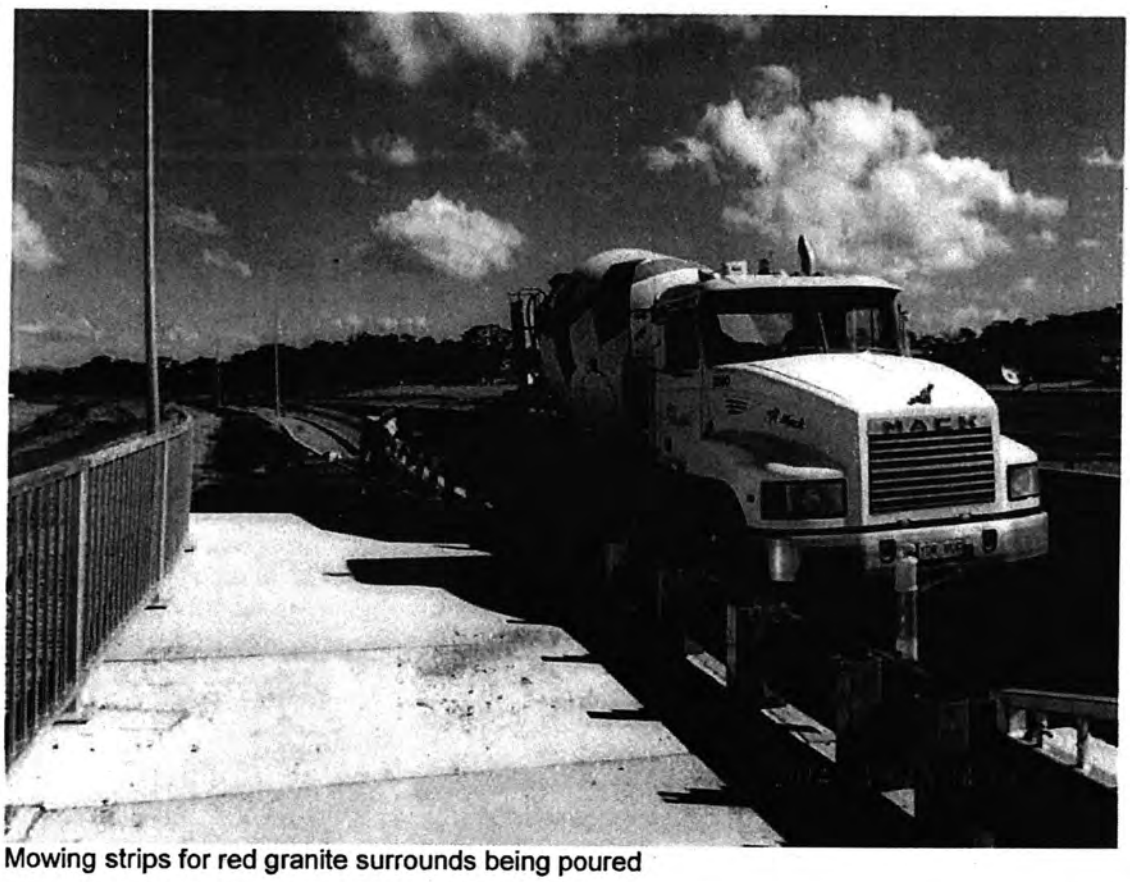
a. Complete placement SMA at Mirrabei intersection;
b. Continue preparation of subgrade and placement of DGB for the shared and equestrian paths under the bridge;
c. Landscape activities to continue across the site;
d. Placement of FGG asphalt on the shared path;
e. Commence placement of decomposed granite under the guard rails at the bridge approaches;
f. Complete concrete infills of traffic islands;
g. Placement of FGG Asphalt for the shared path;
h. Construct pram crossings at the interface between the shared path and carriageway;
i. Commence rectification of defects – cracks in K&G, etc.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos



Progress of various works beneath the bridge



Mowing strips for red granite surrounds being poured

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

WEEKLY REPORT FOR WEEK ENDING: 19 April 2014

1. **WHS**
   a. Site inductions continuing.

2. **Traffic Management**
   a. No changes – construction vehicles using carriageway (with SMA) for access throughout the site.

3. **Design**
   a. *Clarification on TCDs and termination of path adjacent to Whitrod Ave (RFI121A)* – SMEC have raised an RFI to Brown regarding a number of clarifications on the TCDs. Included within the RFI was the question of whether the extension of the path from Whitrod Ave was required.
   b. *Temporary TCD for road stubs* – RD Gossip has prepared a design for the proposed TCDs for the road stubs. EDD and SMEC have raised comments and responded to Gossip. Gossip to seek approval from Roads ACT.

4. **Construction**
   a. SMA placement completed except for three turn lanes at Mirrabei Avenue intersection;
   b. Mowing strip for decomposed granite areas completed;
   c. Construction of equestrian and shared path – preparation of subgrade and placement of DGB commenced;
   d. Pedestrian hand-railing installed at the bridge;
   e. Concrete infill for traffic islands has been completed;
   f. Placement of FGG-R asphalt for the shared path completed from HPD1 interface to CH1140;
   g. Installation of high end riser cover boxes for sub soils completed;
   h. Spreading of topsoil and landscaping activities continuing.

5. **Meetings**
   a. There were no meetings. Next fortnightly site meeting scheduled for 2nd May 2014.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Telstra's has requested for tender for the works (with the requirement that the subcontractor is to be ACT IRE Certified). Tender closes 22 April 2014;
   b. *Excess material potentially requiring disposal* – spreading of stockpile material has commenced and it appears no disposal of spoil of site will be necessary;
   c. *Stone-pitching finishes (ISS022)* – SMEC has issued an instruction to Hewatt to repair the area of stone pitching on 16 April 2014. SMEC has invited Hewatt to dispute this direction under Clause 46.2 – AS2124. Hewatt have 56 days to dispute the direction by issuing a Notice of Dispute.

7. **Payments**
   a. Variations and quantities for Payment Claim No. 13 (April 2014) currently being reviewed and assessed by Hewatt and SMEC.

8. Activities Planned for the Upcoming Fortnight

The site has been shut down for the Easter weekend to end of the Anzac weekend. There may be some construction activities by subcontractors 22nd to 24th April. Construction will recommence on Monday 28th April.

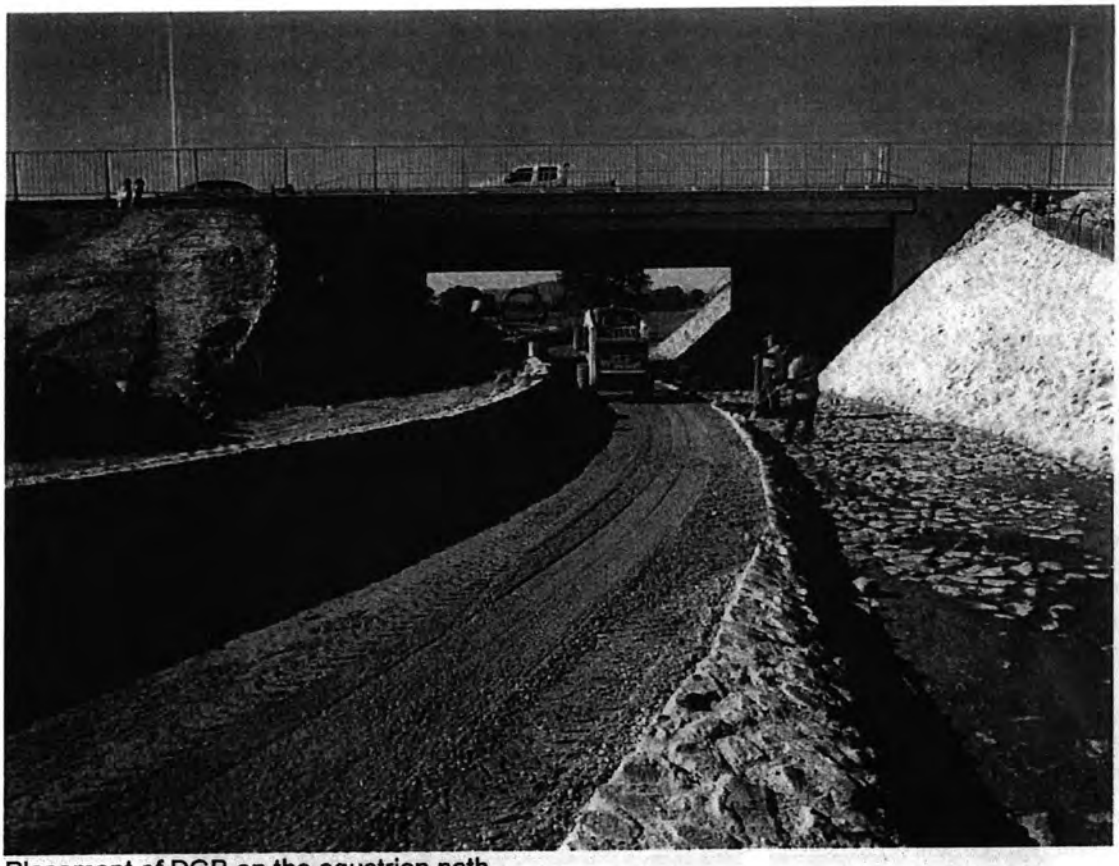
a. Complete placement SMA at Mirrabei intersection;
b. Continue preparation of subgrade and placement of DGB for the shared and equestrian paths under the bridge;
c. Complete stone pitching on the northern side of the bridge adjacent to the shared path;
d. Landscape activities to continue across the site;
e. Placement of FGG asphalt on the shared path;
f. Commence placement of decomposed granite under the guard rails at the bridge approaches;
g. Continue with concrete infill of traffic islands;
h. Placement of FGG Asphalt for the shared path;
i. Construct pram crossings at the between the shared path and carriageway;
j. Commence rectification of defects – cracks in K&G etc.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

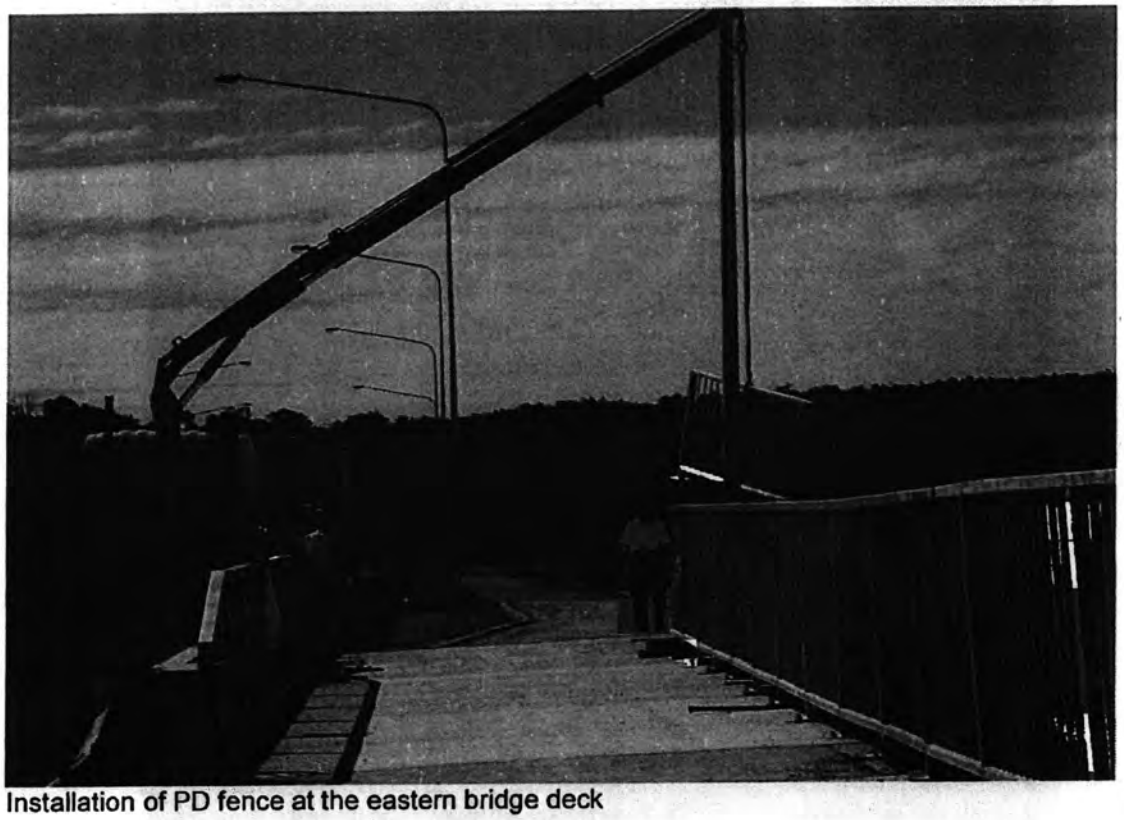
Photos

Asphalt placement Jacka bound carriageway CH1220

Placing stone pitching adjacent to shared path CH60



Placement of DGB on the equstrian path



Installation of PD fence at the eastern bridge deck

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

WEEKLY REPORT FOR WEEK ENDING: 12 April 2014

1. **WHS**
   a. Site inductions continuing.

2. **Traffic Management**
   a. No changes – construction vehicles using carriageway (with SMA) for access throughout the site.

3. **Design**
   a. *Clarification of Asphalt Joints (RFI120A)* – SMEC have issued a response to Hewatt (who have passed the required information onto Downer).

4. **Construction**
   No construction activity occurred on Thursday due to rain, 18mm was recorded to 9.00am Friday and 0.5mm to 5.00pm Friday
   a. Placement of 14/7mm two coat seal completed;
   b. South west mowing strip for decomposed granite poured and form work for the eastern side of the bridge commenced;
   c. Equestrian/shared path retaining wall completed;
   d. Electrical power supply from Casey connected to mini pillars and site documentation / excavation permits updated;
   e. Placement and trimming of DGB for the shared path
   f. Shared path –placement of prime commenced;
   g. Installation of high end riser cover boxes and plates;
   h. Spreading of topsoil and landscaping activities continuing;
   i. SMA placed on the Casey bound carriageway from HPD1 to the bridge, and full width from the bridge to Burrumarra Avenue.

5. **Meetings**
   a. EDD (Glenn Lacey, John Husband and Michael Sergi), Roads ACT (Ben McHugh) and SMEC (and met at the stone pitching to discuss potential ways forward.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Telstra's has requested for tender for the works (with the requirement that the subcontractor is to be ACT IRE Certified). Tender closes 22 April 2014;
   b. *Excess material potentially requiring disposal* – spreading of stockpile material has commenced and it appears no disposal of spoil of site will be necessary;
   c. *Stone-pitching finishes (ISS022)* – EDD, Roads ACT and SMEC met on site. SMEC will issue a CAR for rectification of the stone-pitching (at Contractor's expense).

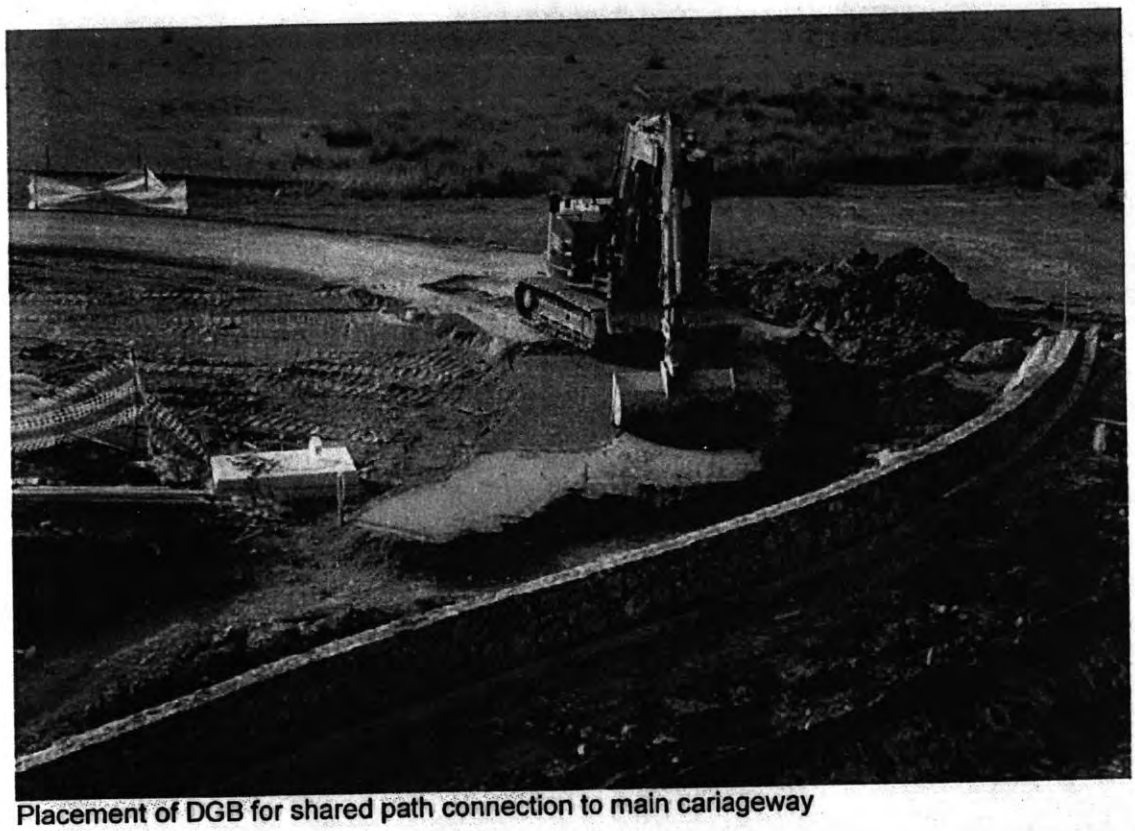
7. **Payments**
   a. Payment Claim No. 12 – March 2014 certified for

8. **Activities Planned for the Upcoming Fortnight**
   a. Complete placement SMA;
   b. Prepare subgrade to design level for the shared and equestrian paths under the bridge;

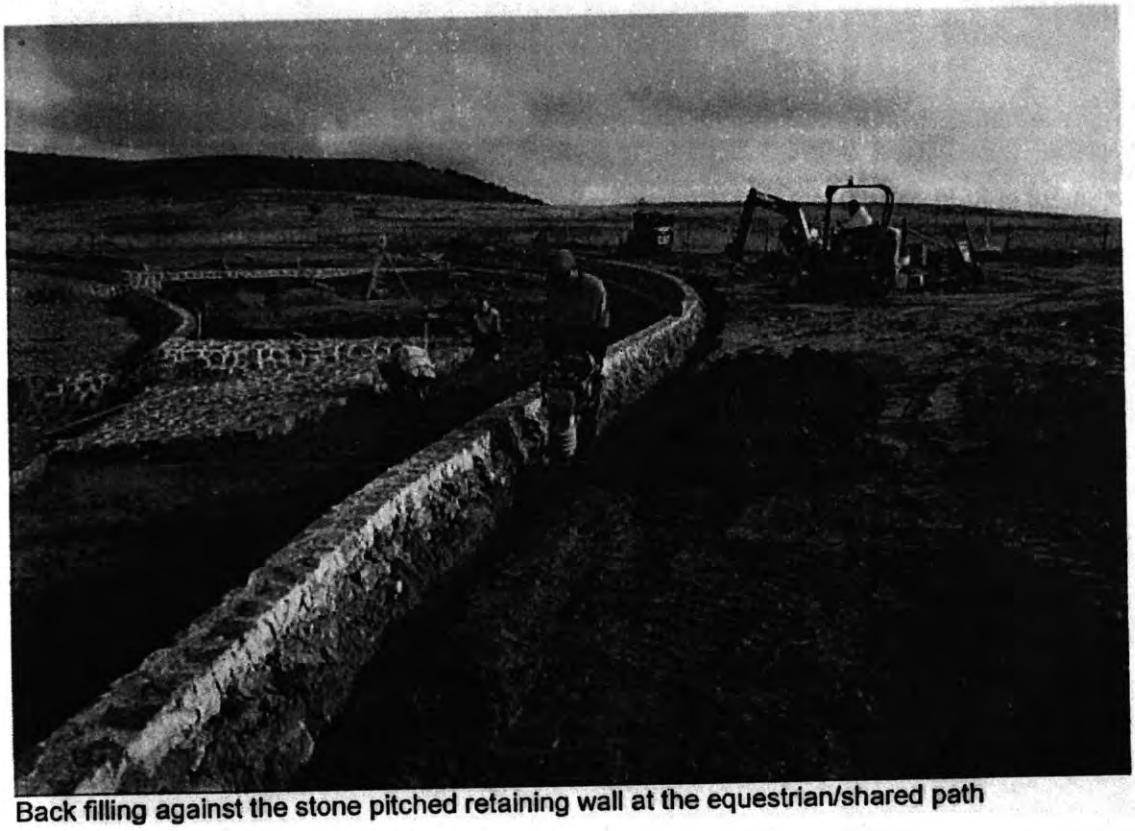
c. Commence stone pitching on the northern side of the bridge adjacent to the shared path;
d. Landscape activities to continue across the site;
e. Placement of prime on the shared path commenced;
f. Continue trimming of DGB to design levels for the base of the shared path;
g. Continue installing concrete covers and lids for sub soil high end and intermediate risers;
h. Complete mowing strips for decomposed granite area under the guard rails at the bridge approaches;
i. Continue with concrete infill of traffic islands;
j. Placement of FGG Asphalt for the shared path
k. R sumps being cleaned and checked for final inspections;
l. Landscaping continuing

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos



Placement of DGB for shared path connection to main cariageway



Back filling against the stone pitched retaining wall at the equestrian/shared path

Placement of SMA CH1650

Placement of SMA CH980

Prime placed on the shared path

Our Ref: C12022 ProgCert18Letter.docx
Your Ref: Horse Park Drive - 19125
Contact:

BROWN

10 April 2014

Hewatt Earthworks
PO Box 94
HALL ACT 2618

Attention:

Dear ,

Horse Park Drive Extension to Moncrieff Group Centre
Progress Certificate #19

We refer to your Progress Claim No. 19 received 9 April 2014 in the value of for a total value of Works of for works completed to 31st December 2013. We have assessed this claim and advise as follows:

Contract Works

» All claimed items are supported

Variations

Variations submitted as part of the current claim are:

» Variation #61 – Additional landscaping between bridge and Jacka Subdivision (works taken from Jacka Contractor) – The approved variation totals , which was approved by the Client on 27 August 2013. For previous claims a percentage complete of 85% has claimed, totaling . The total claimed against the variation for current claim is

Based on the above, please find attached our Progress Certificate No. 19 in the sum of for a total Value of Works to Date of

Please forward a subsequent Tax Invoice directly to the Principal for processing and a copy to this office for our files.

Smart Consulting

Level 6, 121 Marcus Clarke Street, Canberra City ACT 2601
PO Box 261, Canberra ACT 2601 Australia
Telephone +61 2 6211 7100 Facsimile +61 2 6211 7199

Brown Consulting (ACT) Pty Ltd
ABN 36 109 434 540
brownconsulting.com.au

Should you have any queries regarding the above please do not hesitate to contact the undersigned.

Yours sincerely
Brown Consulting (ACT) Pty Ltd

Superintendent's Representative

ATTACHMENTS
1. Progress Certificate #19

COPIES:
1. Patrick Hancock (SSP)
2. John Husband (EDD)
3. (Brown)

BROWN
Smart Consulting

Level 6, 121 Marcus Clarke St, Canberra ACT 2601
GPO Box 261, Canberra City, ACT 2601
Phone 02 6211 7100 Fax 02 6211 7199

Brown Consulting (ACT) Pty Ltd
ABN 36 109 434 540
brownconsulting.com.au

PROGRESS CERTIFICATE

Progress Certificate No: 19 **Contract No:** C12022

Period of Certification: 01-Jan-14 to 31-Mar-14

Project: **Horse Park Drive - Contract Admin**

Contract: 2012.19125.338

Principal: ACT Shared Services Procurement

Contractor: Hewatt Pty Ltd
PO Box 94 HALL 2618

PARTICULARS OF CONTRACT

Contract Value at Date of Acceptance: (Includes GST)

Net Value of Variations Authorised in previous Certificates:

Value of Variations Authorised with this Certificate:
Additions
Deductions
Net Total

Net total of Variations Authorised with this Certificate:

Contract Value as varied to date:

PARTICULARS OF PROGRESS PAYMENT

Value of Work Completed to date:

Less Retention:

Less Security:

Recommended Liquidated Damages:

Sub-total:

Less Payments previously certified:

Amount Certified as due:

Adjustment:

Amount Certified for Payment $

This is to certify that in respect of a claim by the Contractor received on 08-Apr-14 the Contractor is entitled to payment of 1 for work completed on this Contract.

Brown Consulting (ACT) Pty Ltd ...

Date.....10/4/2014.....

Distribution: Principal - original; Contractor - copy; File - copy

CERTIFICATE OF COMPLIANCE FORM USF957

PROJECT: HORSE PARK DRIVE EXTENSION TO MONCRIEFF GROUP CENTRE

CLIENT: Shared Services Procurement **PROJECT No: 19125.130**

SUPERINTENDENT: BROWN CONSULTING (ACT) PTY LTD

CONTRACTOR: HEWATT LTD

SCOPE OF WORKS SUBMITTED FOR ACCEPTANCE: CONSTRUCTION OF HORSE PARK DRIVE EXTENSION TO MONCRIEFF GROUP CENTER INCLUDING CIVIL AND HYDRAULIC WORKS . CLAIM 1A WORK TO 1 MARCH 2014

This section to be completed by Contractor's Representative:

I hereby claim that the above listed works have been duly completed in accordance with the specification requirements, except for the concessions granted in writing by the Superintendent which are detailed in the attached listing nominated in (e) below.

I have verified that:

(a) outstanding minor non-conformances as per the attached listing are applicable to the works.
Applicable: Yes/No (Delete as applicable) Rectification ongoing

(b) all Inspection and Test Plan(s) (ITP s) applicable to the above works, as indicated in the attached register of ITP's, any associated test record sheets and other quality records as required by the specification, have been satisfactorily completed and signed. ✓

(c) any identified non-conformances have been satisfactorily disposed of and their documentation closed out, excepting any minor non-conformances noted in (a) above. ✓

(d) all positive recalls have been completed and verified. N/A

(e) concessions have been granted for the nonconforming items on the attached listing.
Applicable: Yes/No (Delete as applicable) N A

(f) all required documentation, including listing, is attached. Upon request

NAME (PRINT): _ SIGNATURE: _ DATE: 8/4/14
(Contractor's Representative)

This section to be completed by the Superintendent's Representative:

Contractor's above completed works claim is noted.

(a) Payment to be reduced for outstanding minor non-conformances YES " NO "
(Tick As Appropriate)

NAME (PRINT) SIGNATURE: DATE: 10/6/14
(Superintendent's Representative)

BROWN CONSULTING (ACT) P/L

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

WEEKLY REPORT FOR WEEK ENDING: 5 April 2014

1. **WHS**
   a. Site inductions continuing - 211 personnel to date

2. **Traffic Management**
   a. No changes – site vehicles continuing to use main carriageway for access throughout Project (shoulders being utilised during priming and sealing works).

3. **Design**
   a. *Clarification of Asphalt Joints (RFI120A)* – SMEC have issued a response to Hewatt (who have passed the required information onto Downer).

4. **Construction**
   No construction activity occurred on Friday due to rain, 35.5mm was recorded overnight and it continued throughout the day.
   a. Placement of prime completed;
   b. Placement of 14/7mm two coat seal completed from the bridge CH450 to interface with HPD1 CH1800;
   c. North west mowing strip for decomposed granite poured and the form work for the south western side of the bridge completed;
   d. Gas connections were completed and the pipeline is now live and site excavation documentation has been updated – DBYG;
   e. Placement and trimming of DGB for the shared path;
   f. Installation of high end riser cover boxes and plates;
   g. Spreading of topsoil and landscaping activities continuing;

5. **Meetings**
   a. Fortnightly site meeting held Friday 4th April.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Telstra to respond to Hewatt with updated price for works with IRE certified contractor;
   b. *Excess material potentially requiring disposal* – it appears there may be a small amount of material that will not be able to be disposed on-site (<1000m3). Hewatt are organising a bulldozer to start regrading areas to lose the spoil;
   c. *Stone-pitching finishes (ISS022)* – SMEC have asked Hewatt for survey information showing conformance to ACT Specifications.
   d. *Traffic lighting at Mirrabei Dr* – SMEC are in correspondence with Roads ACT regarding the completion and handover of the traffic lighting at Mirrabei Dr.

7. **Payments**
   a. SMEC and Hewatt will work together to have Hewatt's Tax Invoice for Payment Claim No. 12 – March 2014 submitted by Friday 11 April 2014.

8. **Activities Planned for the Upcoming Fortnight**

   a. Complete placement of 14/7mm two coat seal;
   b. Complete the retaining wall between the shared and equestrian paths under the bridge;
   c. Landscape activities to continue across the site;

d. Connection of power supply from ActewAGL's mini-pillars to the streetlights and traffic lights;
e. Continue trimming of DGB to design levels for the base of the shared path;
f. Continue installing concrete covers and lids for sub soil high end and intermediate risers;
g. Complete mowing strips for decomposed granite area under the guard rails at the bridge approaches;
h. Continue with concrete infill of traffic islands;
i. Placement of FGG Asphalt for the shared path
j. Commence placement of SMA stating in HPD1.

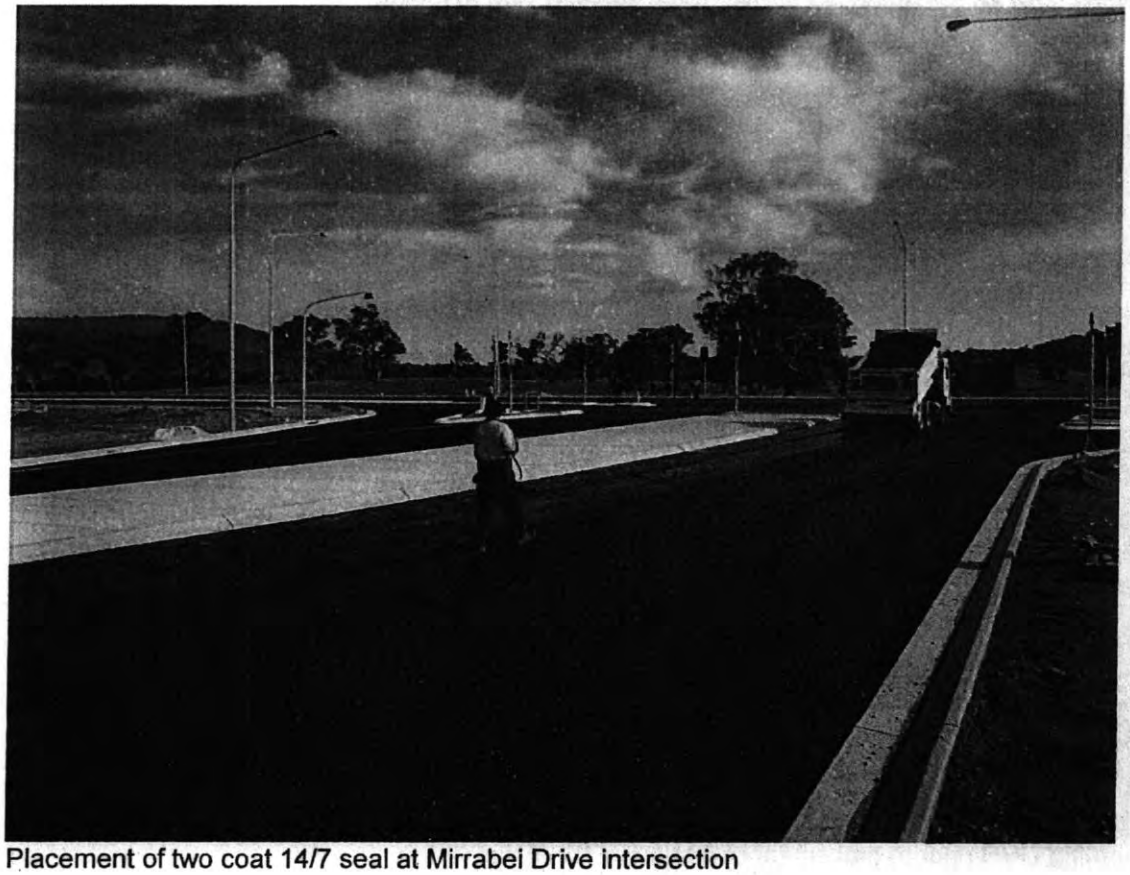
Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos

Mowing strip for decomposed granite north western side of bridge

Brooming DGB prior to placement of prime

Spray seal being placed at CH850



Placement of two coat 14/7 seal at Mirrabei Drive intersection

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

MEETING: SITE MEETING - MINUTES

LOCATION: Project Site Office, Horse Park Drive, Jacka
DATE: Friday, 4 April 2014
TIME: 10.30am
ATTENDEES:

Economic Development Directorate: *Michael McGrath (MM)*
John Husband (JH)
Michael Sergi (MS)

Shared Services Procurement: Patrick Hancock (PH)
James Cargill (JC)

Brown:

SMEC:

Hewatt:

No.	Item Description	Action
01.	WHS • There have been 2 incidents (non-notifiable) since the last site meeting. Both involving unauthorised vehicles entering the site. A police car and an ACT Government (LDA) were stopped and told to exit the site unless the required WHS inductions/sign-ins were completed. • No reports or draft reports have been received from either the Project or the ACT Government from Kaizen/ in relation to the follow up WHS audit held on 17 March 2014. • forwarded on ACT Govt/EDD's Monthly WHS Checklist (to be completed by the Contractor) to Hewatt. It was decided the checklist would be submitted monthly with the claim. • asked if the WHS Guidelines used by ACT Govt would be sent out to Contractors and Consultants. It was noted that the Checklist to be completed by the Contractor was Appendix 5 of the entire Interim WHS Guidelines. JH will enquire to see if these Guidelines will be made available to Contractors and Consultants.	Kaizen/SSP Hewatt JH
02.	Review Previous Minutes / Matters Arising • The Project will continue to keep EDD Communications informed of an approx. completion/road opening date – for the purposes of an opening event. JH will act as the liaison between the Project and EDD Communications. JH has advised that the completion date will likely be in May 2014.	JH
03.	Design • RFIs: 120 submitted; 0 open • JH sent an email with responses to queries regarding the use of RMS R121 Specification. JH will respond to query regarding the performance requirements in Clause 1.1 and their application at intersections. • had sent MM/EDD an email with comments on the use of SMA at intersections/high stress locations. The use of SMA at intersections in ACT is common, however it is avoided in NSW.	JH

	• noted that the query from (& SMEC) regarding Clause 1.1 was not raised in order to propose an alternative asphalt type at the intersection, but more to change the wording of the clause such that it was fair for the Contractor. noted that if the performance requirements within Clause 1.1 were not met in areas other than intersections, there would still likely be assessment/ investigation into the cause and the Contractor would be unlikely to claim full responsibility without a cause being established or an investigation being carried out. • It was noted that the RMS Specification has to account for large road networks (eg. Hume Highway, etc.) with high traffic flow (and heavy vehicle networks) whereas the majority of the ACT's road network would not be as heavily trafficked as these roads. • JH noted that he had responded to the LDA re: Action Bus Stops on HPD – ie. they will not be constructed as Action raised interest too late in the construction process. • The temporary kerbing arrangement intended for CH60-90 will be reviewed following SMA works. • to raise RFI to Brown asking whether the concrete path adjacent to Whitrod Ave (from Springbank Rise) should be extended as part of HPD2's works. There is no provision for extending the path in the current design. • will confirm a few more details on the chase required at Burrumarra Ave (RFI120A)	
04.	Site Compound • Looking to remove Site Compound last week of April 2014.	
05.	Program • to send through updated program showing revised dates for completion (following wet weather). • 75% of seal has been placed, CH60-bridge (and SAMI seal on bridge to be completed). These are planned to be completed on Tuesday 8 April. • noted Downer will commence SMA works on Tuesday 8 April – starting at HPD1 and working up towards HPD2. There will only be 1 crew working until HPD2 completed. SMA works may take 8-10 days. • EOT No. 15 & 16 have been submitted by Hewatt. **Current PC Dates** SP1 & SP3 – Civil, Burrumarra Ave – HPD1 – 03 Apr 2014 SP2 & SP4 – Landscape, Burrumarra Ave – HPD1 – 07 Oct 2014	
06.	Environmental • noted that two representatives from ACT EPA visited the site on 25 Mar 14 to inspect the set up and ensure that works were being completed in accordance with the waterway licence. They were very happy with the set-up, looking particularly at the floodway. • noted that at the meeting with Lend Lease, (Lend Lease) accepted that silt from Springbank Rise (SBR) had been washed into HPD2's floodway – offered to clean this up for Hewatt. • noted that with the strike from the grass in SBR's floodway, the water coming from the Headwall was more of a concern (it was not clean). This water would pick a lot of the material left on the road by builders.	

07.	Industrial Relations • **Raising Telstra Pit at CH40:** noted that Telstra had identified a suitable contractor (with ACT IRE Certification) to complete the works. Contractual arrangements were being made. Telstra will inform Hewatt of any changes to the previously submitted quote. o Telstra are hoping to get back to Hewatt in the next week – completing the works may take an additional ~ 2 weeks. o noted the civil works by Hewatt surrounding the Pit will only take ~2 weeks to complete. o will take notes on Telstra's resources and time spent to complete the Pit works.	
08.	Traffic • has been in correspondence with Cameron Glassford regarding preparations for the operation of the traffic lights at Mirrabei Dr. • Power to the mini-pillars is due to be turned on next week (week starting 7 Apr. 14.	
09.	Community • , & met with : of Lend Lease (Casey – Springbank Rise) to discuss interface issues – particularly relating to handover of HPD2. • asked if there were any requirements in terms of "handover"/completion of works with the adjacent Leaseholders (ie. the stockproof fence constructed by the Project is a shared fence). JH noted that there weren't any requirements. All fencing has been installed in accordance with cadastral boundaries. • to set up meeting with Ngunnawal 2C Superintendent/ Contractors.	
10.	Staffing • No changes to staffing. still supervisor/foreman and will hopefully be on-site until the completion of asphalting works.	
11.	Utilities • Streetlighting: Ecowise will send over WAE such that BEPCON can complete inspection prior to ActewAGL turning on the power. Power will be hooked up from mini-pillars to streetlight controllers on Wed 9 Apr. 14. • Ecowise will organise signals and streetlight inspection prior to power up. • ActewAGL: will be looking to turn on power on 7 Apr. 14 • Actew Water: has sent through WAE to Actew Water and will follow up with a phone call. • ZNX/Gas: Gas has been connected at both ends of HPD2. Live gas is now running through the site. Hewatt's request for permit to excavate was updated accordingly. • NBN Co.: Still awaiting shared trench WAE prior to handover inspection, etc. • Designers of Moncrieff West (Browns) have been asking for WAE for hydraulics and shared trench at Mirrabei Dr and Road 04. • SMEC & EDD are in correspondence regarding the inspection and certification of stormwater works for Capital Works (incl HPD2).	

12.	Issues • in correspondence with Gabriel Joseph regarding minor kerb cracking and proposed rectifications. The intention is to align the rectification measures required by the Superintendent and TaMS. • SMEC will issue a draft Defects Register by the end of the day. • **Handover Inspections:** asked whether there should be a preliminary joint walk through (with all parties), such that Practical Completion could be achieved ASAP after SMA placement. The prelim walk through could be used for the purposes of identifying large defects, etc. and the final walk through could be used to tick off completed defect work and potentially pick up any new minor defects.	SMEC
13.	EOT • EOT Status (16 submitted): 63.5 Days Claimed; 36.5 Days Granted • submitted EOT 15 and EOT 16 which are being assessed by SMEC. EOT 15 – 5 days due to design changes; EOT 16 – 6 days due to wet weather. • JH noted that there haven't been too many discussions re: date for PC and date of PC (& potential LD's). sent through an email with reference to a number of potential EOTs.	
14.	Variations • **Status** (including BoQ over/underruns) (excl GST): 7 Submitted; Endorsed; 1 Certified for payment (as at end of Feb '14) • noted that SMEC's assessment of VO.38 was that the price submitted by Hewatt was considered fair and indicative of current industry rates. PH/JH agreed and said they would send an email to note this. • SMEC are still awaiting a revision to VO.31 to include all works associated with the electrical supply to the streetlight and traffic light controller. • noted that there will be a number of Variations submitted with the March 2014 claim for design changes – mainly around the bridge.	JH/PH
15.	Progress Claims • will prepare March 2014 claim and SMEC and Hewatt will work together to ensure the Tax Invoice is submitted to PH prior to 16 April 2014.	
16.	Quality Assurance • HPs: 322 raised; 10 open • NCRs: 30 raised (25 Quality; 5 Safety) • Hewatt will update the resolution to the bridge deck NCR once survey of the SAMI seal has been taken. o noted that many bridge designs include an additional asphalt layer on the bridge to allow a profiler to remove the entire depth of wearing course in future maintenance. This would also alleviate some of the issues we have with the • Hewatt have submitted HP's for SMA10 and prime/spray seal designs. noted that these Hold Points have not been released yet as SMEC are awaiting information from Hewatt. noted that the Mix/Prime/Spray Seal Designs were all OK, however not all supporting documentation has been submitted.	

	• There are still a number of Hold Points not released due to insufficient information	
17.	Working hours and days • noted that work would continue on two non-lockdown RDOs between Easter and ANZAC Day.	
18.	Construction Matters • JH noted that RD Gossips were on the case – to provide stub termination details. JH to follow up. • . to complete pavement profile report following completion of SMA/seal. • MS noted that there was an area next to the SFK at the interface to HPD1/2 that had not been primed or sealed. MS asked what the intended treatment was. noted this was within HPD1 revised Limit of Works. • noted that Hewatt must protect landscaping works through consolidation. This includes rectifying damage to verges by vehicles (which is very common). The only other alternative was to erect bunting and star pickets beside the road (which is a safety risk). The issue was noted.	JH
19.	Subcontractors • sent contract for Downer through to	
20.	Next Meeting(s) • Friday, 2 May 2014 – Site Meeting – this will depend on the state of the Project at the time. . to confirm.	
21.	Other?	

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

WEEKLY REPORT FOR WEEK ENDING: 29 March 2014

1. **WHS**
   a. Site inductions continuing.

2. **Traffic Management**
   a. No changes – site vehicles continuing to use main carriageway for access throughout Project (shoulders being utilised during priming and sealing works).

3. **Design**
   a. *Clarification of Asphalt Joints (RFI120A)* – Hewatt have raised an RFI seeking clarification of joints in asphalting above bridge expansion joints and at the tie-in to existing pavement at Burrumarra Ave.

4. **Construction**
   Inclement weather for the week forced the cancellation of any construction activities. A total of 60mm was recorded and the installation of traffic light lanterns only new work commenced, apart from the maintenance of erosion and sediment controls.

5. **Meetings**
   a. Two representatives from the ACT EPA inspected the site on 25/3/14. They were satisfied with the erosion and sediment controls in place and there were no adverse comments or findings.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Telstra met with Hewatt on-site throughout the week to work through the issue. Telstra are working to find an ACT IRE certified contractor;
   b. *Excess material potentially requiring disposal* – it appears there may be a small amount of material that will not be able to be disposed on-site (<1000m3). This is to be confirmed as finished surface levels are finalised;
   c. *Interface with Springbank Rise (ISS002)* – Hewatt & SMEC met with Lend Lease to talk about a range of issues, including completion;
   d. *Stone-pitching finishes (ISS022)* – SMEC have asked Hewatt for survey information showing conformance to ACT Specifications.
   e. *Traffic lighting at Mirrabei Dr* – SMEC are in correspondence with Roads ACT regarding the completion and handover of the traffic lighting at Mirrabei Dr.

7. **Payments**
   a. Brown forwarded results of model analysis of earthworks quantities for SMEC to use in quantity agreement;
   b. SMEC and Hewatt will work together to have Hewatt's Tax Invoice for Payment Claim No. 12 – March 2014 submitted by Friday 11 April 2014.

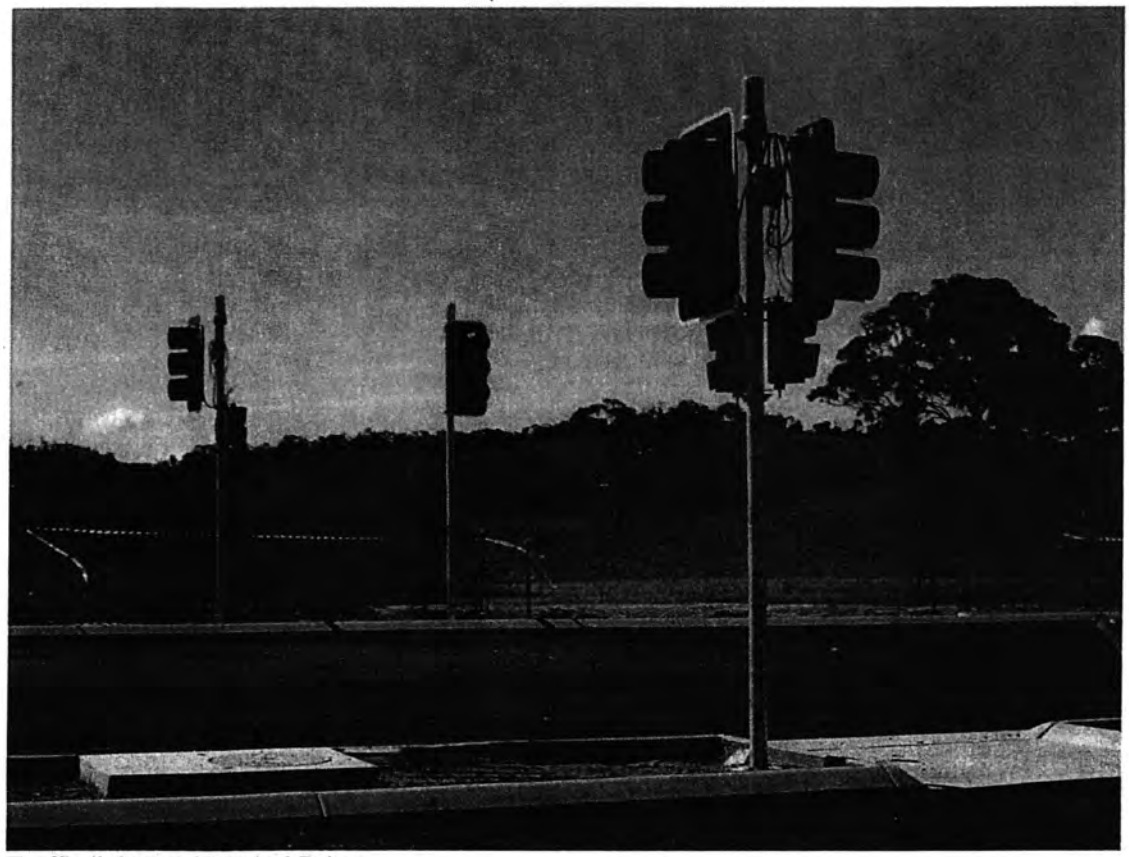
8. **Activities Planned for the Upcoming Fortnight**

   a. Place prime on the remaining section completed base CH50 to CH880;
   b. Commence placement of 14/7mm two coat seal;
   c. Complete footing for retaining wall between shared and equestrian paths under the bridge and continue construction of the wall;
   d. Landscape activities to continue across the site;

e. Connection of power supply from ActewAGL's mini-pillars to the streetlights and traffic lights;
f. Continue trimming of DGB to design levels for the base of the shared path;
g. Construct flushing points for sub soil high end and intermediate risers;
h. Complete mowing strips for decomposed granite area under the guard rails at the bridge approaches;
i. Continue with concrete infill of traffic islands.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos

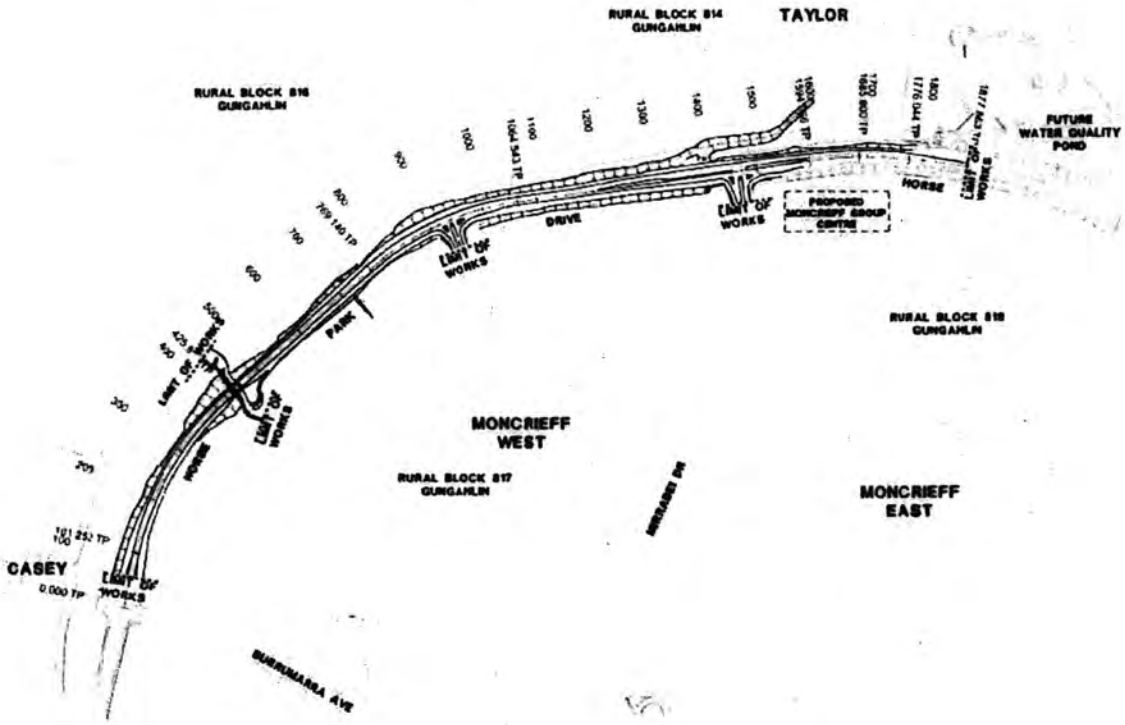


Traffic lights at Mirrabei Drive

Construction of:

Horse Park Drive Extension Project
Burrumarra Avenue to Mirrabei Drive

Monthly Report



Project:	**Horse Park Drive Extension (Burrumarra Ave – Mirrabei Dr)**	
Construction Contract No:	**2013.21048.110**	
Report No:	**12**	
Reporting Period:	**March 2014**	
Senior Program Manager:	Michael McGrath	*Economic Development Directorate*
Senior Project Manager:	John Husband	*Economic Development Directorate*
Senior Project Officer:	Patrick Hancock	*Shared Services Procurement*
Contractor's Representative:		*Hewatt Pty Ltd*
Superintendent's Representative:		*SMEC Australia Pty Ltd*

ACT Government

HEWATT
CIVIL ENGINEERING CONTRACTORS

SMEC

Table of Contents

1. WHS

Three days Lost Time Injuries (LTI's) were reported for March 2014. These are the first LTI's recorded for the project to date.

There were two WHS incidents (non-notifiable) recorded for March 2014.

A follow up ACT WHS Audit was conducted by Kaizen Management Services on Monday 17 March 2014. Results of the report are yet to be released.

SMEC's surveillance officer has continued to carry out a joint walk-through / safety inspection on a weekly basis with the Contractor's Safety Officer or Site Foreman.

The WHS Incident Register is included at Appendix A.

2. Progress of Work

The month of March 2014 included 21 contractual working days with a single RDO. Progress within the month was significantly affected by wet weather with seven working days being impacted by over 70mm of rain. This included an entire week where no work was carried out.

The remaining 14 working days saw progress continue in the preparation of the carriageway and trunk cycle path for priming, sealing and asphalting works. Work also continued on stone pitching, retaining walls and path foundations beneath the bridge. The works beneath the bridge have now formed somewhat of a critical path of their own.

Other items that affected progress throughout the month of March 2014 included delays in the construction of the stone retaining wall footing beneath bridge due to design changes and the delay in Telstra engaging a suitable subcontractor to carry out works to the existing pit at Burrumarra Ave.

As at the end of March 2014, the date for Practical Completion for Separable Portions 1 & 3 is 3 April 2014. An additional 11 days EOT have been submitted Hewatt for assessment by SMEC. Given the current state of the Project, it is unlikely the date of Practical Completion will be prior to the 3 April 2014.

A summary of the work completed in March 2014 is provided in Sections 2.1 and 2.2.

2.1. Separable Portion 1 & 3

Bridgeworks:

- Hand railing across bridge installed – 100% Complete;

Pavement:

- Base placement was completed from Burrumarra Ave to the tie-in to HPD1;
- Prime applied to 50% of carriageway from CH880 to tie-in to HPD1;
- Kerb placement completed across site;
- Concrete poured for traffic islands – 80% Complete;
- Trunk cyclepath construction:

- Subgrade preparation – 90% Complete. Subgrade preparation surrounding Telstra pit still to be completed;
- Base placement – 75% Complete

Carriageway & Road Furniture & Misc.:

- Streetlighting installation of poles and lanterns – 95% Complete. Streetlight controller and lights beneath bridge to be completed;
- Placement of base material for Ginninderra Red Granite surrounding guardrail commenced;
- Guardrail completed.

Floodway & Beneath Bridge:

- Stone pitched retaining wall 90% Complete;
- Area of stone pitching previously removed has been replaced
- Earthworks and set out for equestrian and shared path adjacent to floodway continuing.

Underground Services:

- Finishing works to stormwater works completed (including installation of sump covers);
- Concrete base for Traffic Light Control Cubicle completed;
- Electrical supply connected to streetlight and traffic light locations (not energised);

Landscaping:

- Topsoil placement continued along the verges of the road. Topsoiling works are approx. 95% complete;
- Bitumen Straw Mulching continued on-site – 75% Complete.

2.2. Separable Portion 2 & 4

- Landscaping across the site is continually being carried out.

3. Issues and Project Risks

3.1. Key Issues

Current issues include:

a. **Springbank Rise (SBR) Interface (ISS002):** The current status of the various issues surrounding the Project's interface with the Springbank Rise Development are:
   - SMEC and Hewatt met with Lend Lease within March to discuss completion items, including treatment of landscaping surrounding the Casey Headwall.

b. **EDD Report on SMA within HPD1 & Outcomes for HPD2 (ISS018):** The SMA wearing course layer has been changed to SMA10 at a nominal thickness of 30mm. Hewatt has constructed the base material layer to the revised level and has submitted a Variation claim for the changes to the SMA.

c. **Stone Pitching on the Western Bridge Embankment:** A section of the stone pitching was removed and replaced by Hewatt. In early March 2014, EDD requested that this area was not large enough and requested additional areas to be removed.

SMEC have sent a formal instruction to Hewatt to replace the enlarged section or to formerly dispute the instruction;

d. **Raising of Telstra Pit at CH40:** Telstra are required to raise an existing pit at CH40 adjacent to the trunk path. Telstra initially engaged a subcontractor that was not IRE certified. Telstra are currently looking for an applicable IRE certified subcontractor.

The Issue Register has been included in Appendix C

3.2. Key RFI's

There were a total of four (4) RFIs raised in March 2014. A short summary of the nature and response to the significant RFIs within March is included below.

- **RFI109A/RFI117A: Clarification of revised cross-falls provided as part of RFI109A:** the revised design model issued as part of the response to RFI109A included cross-falls of cyclepaths ~5% at the interface of the concrete and asphalt path adjacent to the floodway. RFI117A was raised asking whether the cross-falls given were acceptable or whether they needed to be revised. The levels were subsequently revised;
- **RFI112A: Confirmation of changes to pavement design:** All changes to the SMA wearing course and specification have been confirmed by SMEC/EDD. Hewatt have submitted a variation for the changes;
- **RFI120A: Clarification of asphalt joints:** Hewatt raised an RFI asking how the SMA wearing course is to be transitioned and "keyed" into the existing asphalt at Burrumarra Ave. SMEC has since responded to the RFI.

The RFI register is included at Appendix F.

4. Time

The Letter of Acceptance was issued to Hewatt on 26 February 2013.

The initial construction period was for 62 weeks, with a landscaping consolidation period of 26 weeks, totalling 88 weeks. The Initial contractual Practical Completion (PC) date was 04 November 2014 (including Landscape Consolidation).

As the result of Hewatt's proposal to align the completion of HPD1 & HPD2, the construction period has been revised to 56 weeks with a consolidation period of 26 weeks. The current date for PC for SP4 is 7 October 2014.

4.1. Program and Progress

The latest revision to the construction program was received on the 14th February 2014.

Table 4.1: Programmed completion dates for Separable Portions 1 & 3

Portion	Description	Original PC Date	Revised PC Date	Current Program Completion Date
SP1	Burrumarra Ave – Ch 600	03 Dec 2013	3 Apr 2014	2 Apr 2014
SP3	Ch 600 – HPD1 Interface	06 May 2014	3 Apr 2014	2 Apr 2014

The latest construction program issued is outdated. A revised program to completion has been requested from Hewatt by SMEC. This program is still being awaited.

There have been three EOT Claims raised in March 2014, with only EOT No. 14 being accepted by SMEC as yet. SMEC intends to accept the additional 11 days claimed within EOT No. 15 & 16 shortly. Whilst this will push the date for PC to 28 April 2014, the completion of the Project to the state required by Clause 14.06 SCC will not occur prior to this date. It is expected that the date for PC will be mid-May 2014.

A detailed outline of the works still to be completed is included in Section 11.

The latest Contract Program to Completion from Hewatt is included at Appendix H.

4.2. Extension of Time (EOT)

To date, sixteen (16) EOT claims have been submitted by Hewatt. Two EOT Claims were raised in March 2014 due to inclement weather and EOT 16 was raised regarding delays due to design changes. Hewatt have advised that there will be no cost associated with EOT 16 if approved.

A total of 63.5 days have been claimed by Hewatt, with 36.5 days being approved. As noted previously, SMEC intend to approve all time claimed within EOT No. 15 & 16.

The current dates for Practical Completion are set out in the table below.

Portion	Description		Original Date	Revised Date
SP1/3	Burrumarra Ave – HPD1 Interface	Civil Works	03 Dec 2013/ 06 May 2014	3 Apr 2014
SP2/4	Burrumarra Ave – HPD1 Interface	Landscaping	03 Jun 2014/ 04 Nov 2014	7 October 2014

The EOT Register is included at Appendix B.

5. Financial Position

Financial position to the end of March 2014 is reported below:

5.1. Progress Payments

Progress Payments certified to the end of March 2014:

Original Contract Value including Provisional Sums:

Approved Variations (incl. Quantity Overruns):

Current Approved Contract Value:

Value Certified To Date:

Value Previously Certified:

Progress Payment Claim No. 12 for March 2014:

NB: all amounts are inclusive of GST.

The following table shows Progress Payment Certifications to date:

Progress Payment No.	Month	Payment Certified (including GST)
1	Apr-13	
2	May-13	
3	Jun-13	
4	Jul-13	
5	Aug-13	
6	Sep-13	
7	Oct-13	
8	Nov-13	
9	Dec-13	
10	Jan-14	
11	Feb-14	
12	Mar-14	
	Total to Date:	

Figure 5.1: Hewatt's Original and Revised Forecast Claim against Actual Claim, as at 31 March 2014.

5.2. Claims and Variations

SMEC's Variation Register is included at Appendix D.

There have been a total of 38 Variations raised to date. The total agreed value for Variations is currently with being certified as at the end of March 2014.

The significant Variations submitted to date include (all figures excl GST):

- V06 – Changes to Watermain Design
- V09 – Pricing of Missing BoQ Items
- V29 – HPD1/2 Change in Scope
- V38 – Changes to SMA Wearing Course
- All other Variations (incl. over/underruns)

5.3. Estimated Final Contract Value

The Estimated Final Contract Value is being revised each month as the Project progresses. The Estimated Final Contract Value is continually tracked using the following methods:

1. Tracking of Provisional Quantity Items and updating estimated final quantities monthly (refer to *Table 5.3.1*);
2. Tracking of Provisional Sums and updating estimated final amount of each Provisional Sum following each claim (refer to *Table 5.3.2*);
3. Tracking overruns/underruns in quantities of Scheduled Items (refer to *Appendix E*);
4. Incorporating changes in the design and construction into the Estimated Final Contract Value summary table (refer to *Appendix E*). This will also generally include forecast Variations that have not yet been submitted by the Contractor (eg. additional wearing course works at Burrumarra Ave).

Hold Point Register Status	As at March 2014	NCR Register Status	As at March 2014
No. Raised	322	No. Raised	31
No. Open	**10**	**No. Open**	**7**

8. Traffic

At the commencement of the Project the site was fenced off to public pedestrian and vehicular access. The only interactions with public traffic is upon entry and exit from the site.

The main entry to the Site Compound is now adjacent to Amaroo School. There have been no incidents with public traffic. It has been noted that a small number of vehicles have inadvertently entered the site through this entry. These vehicles were turned around prior to the site compound.

9. Community

Hewatt and SMEC met with : from Lend Lease in March 2014 to discuss completion of works at the interface of the two projects.

The only other public liaison throughout March was by John Husband (EDD) responding to a phone call asking about the completion date of Horse Park Drive from Amaroo to Burrumarra Ave.

The Public Liaison Register can be found at Appendix G.

10. Industrial Relations

The only issue concerning industrial relations in March 2014 was regarding Tesltra trying to find a suitable IRE accredited subcontractor.

11. Upcoming Works

The following works are yet to be completed by Hewatt. These works will be completed within April and May 2014.

Pavements:

- Completion of prime, seal and SMA works for the main carriageway;
- Completion of base placement, prime and FGG for the trunk cyclepath;
- Pavement marking and road sign installation across site.

Bridgeworks & Floodway:

- Completion of all associated bridgeworks (incl. handrailing, joint sealing & anti-graffiti painting;
- Completion of stone-pitched retaining wall beneath bridge and adjacent equestrian and shared paths;
- Completion of stone pitching for bridge abutments;
- Completion of the equestrian and shared concrete paths beneath the bridge.

Services:

- Complete WAE for services and handover to ACTEW Water, NBN Co. and other relevant authorities.

Road Furniture & Misc.:

- Completion of street-lighting and traffic lighting (and associated control cubicles);
- Completion of guard-railing either side of bridge.

Landscaping:

- Completion of topsoiling, bitumen straw mulching and planting across site;
- Planting of trees across site.

Appendices

A. WHS Incident Register
B. EOT Register
C. Issues Register
D. Variation Register
E. Estimated Final Contract Value Summary
F. RFI Register
G. Public Liaison Register
H. Contract Program
I. Photographs

Appendix A. WHS Incident Register

INCIDENT/NEAR MISS REGISTER - ALL

SMEC

ID No	Type	Date	Location	Incident Description	Parties Involved	Consequences of Incident	Action Taken	Status
1	WHS Incident	10/05/2013	Site: CH0-500	A member of the public was seen walking on the site	Member of public, Hewatt	No injuries.	Signage and fencing at Site Entry	Closed
2	WHS Incident	23/05/2013	CH450	The temporary site fencing on the Casey Development side of the floodway was damaged/cut open.	Unknown	Damage to fencing.	Fencing was reinstated.	Closed
3	Observation	24/07/2013	Temporary floodway (CH420)	The sandbags used for shoring the access road over the temporary floodway were noticed to be slightly displaced. This was caused by settlement under loading of the access road. If left this could be a potential problem.	Identified by SMEC	N/A	The road crossing has been rebuilt and the sandbags replaced in a more secure formation. Ongoing monitoring will be undertaken.	Closed
4	WHS Incident	11/09/2013	CH1400 - Sewer manhole	Worker placing concrete for sewer manholes was working at heights without adequate safety measures	ACT Civil & Subcontractor	No injuries or accidents.	Hewatt advised ACT Civil of the incident and the work methods being used on site to pour the manholes was adjusted accordingly.	Closed
5		14/10/2013	SP3	Operative Hurt his back when changing the cutting edge on the scrapper	Hewatt		An incident report was completed.	
6	Environmental Incident	15/10/2013	Road 04 Intersection	A dump truck had an engine failure at approx CH840. The truck came to a safe stop with no other damage or injuries.	Machine Operator	Oil and coolant spilt from the container and onto the road formation. This was cleaned up by Hewatt. There was no other damage or injuries.	Oil Spill was cleaned up by Hewatt and the incident was recorded	Closed
7	WHS Near Miss	16/10/2013	Embankment adjacent to Western Abutment	A compactor was working close to the edge of the steep embankment - the machine's front wheels got stuck on the edge of the embankment. The compactor was not meant to be working this close to the steep embankment.	Compactor Operator	The compactor required assistance to get out of the situation.	The occurance was recorded as a near miss due to operator error.	Closed
8	WHS Incident	20/11/2013	HPD1-HPD2 Site Compound	Vandals broke into the site and caused significant damage to the site compound (broken windows), equipment and machinery (water truck graffitied).	Unknown vandals	No injuries or harm was caused to site personnel.	Police examined the vandalism and lodged a report.	Closed
9	WHS Incident	13/12/2013	Block 818	A fire was started in the unleased territory Block 818. The fire is thought to have been caused by mowing that took place within the block.	LDA mowing contractor; Hewatt Site Staff	No injuries or harm was caused to site personnel.	Hewatt attended the fire with fire extinguishers and a water cart. In January 2014, LDA inspected the mowing and Hewatt informed the representatives of the fire. LDA were to investigate.	Closed

ID No	Type	Date	Location	Incident Description	Parties Involved	Consequences of Incident	Action Taken	Status
10	WHS Incident	3/02/2014	HPD1 - Amaroo School to Jacka Access 1	A cyclist was identified riding through the asphalted stage of HPD1, within the Limit of Works for HPD1 (which is also used as the entry to HPD2).	Cyclist, Hewatt	No parties were injured.	The cyclist was escorted from the Site by Hewatt staff.	Closed
11	WHS Incident	5/03/2014	Guardrail beside bridge	Operator was manouvering the post driver into position and ran over his own foot, with the safety bar hitting his shin.	Euro Civil; Hewatt	Swelling and bruising evident on shin and swelling, bruising and numbness in foot. Operator taken to casualty and was discharged. There were 3 days LTI.	Operator taken to casualty and Hewatt completed incident report for recording.	Closed
12	WHS Incident	24/03/2014	On-site	Hewatt workers sitting on tray of ute as it was in motion - dropping sandbags off the back prior to the rain event.	4 x Hewatt Site Staff	No injuries.	Workers were addressed and recognised they were operating outside Hewatt's Vehicle Operation Policy.	Closed
13	WHS Incident	1/04/2014	On-site	A police car had driven through the site without signing in or notice to any site staff.	ACT Police	No injuries.	ı stopped and advised the police car that if they wish to drive through the site, they need to have an induction and sign in.	Closed
14	WHS Incident	2/04/2014	On site	An ACT Government vehicle was attempting to drive through the site to have a look at Taylor and Moncrieff. The car did not stop at the site offices.	ACT Government Car	No injuries.	er stopped the car and asked the car to leave site - there was no access to through the site as the spray seal was being placed. They were also informed that they needed to sign in and be inducted to go through site unaccompanied.	Closed

Appendix B. EOT Register

HORSE PARK DRIVE EXTENSION – BURRUMARRA AVE TO MIRRABEI DR Contract No: 2013.20148.110

EOT Register

Date of Issue of Letter of Acceptance	26-February-2013			
Separable Portion No.	SP1	SP2	SP3	SP4
Original Duration (Construction Period + Consolidation Period)	40	66	62	88
Original Date for Practical Completion	3-Dec-13	3-Jun-14	6-May-14	4-Nov-14

EOT Number	Date Raised	Contract Issue Number (if applicable)	Description	Raised By	EOT Claimed (No of Days)	EOT Granted (No of Days)	Revised Date for Practical Completion SP1	SP2	SP3	SP4	Date Hewatt Advised	Status
01	17/04/2013	N/A	Delays to works as a result of For Construction documentation not being provided in a timely manner.		14	0	3/12/2013	3/06/2014	6/05/2014	4/11/2014	17/06/2013	Closed
02	17/04/2013	N/A	Delays to works as a result of late receipt of Letter of Acceptance		2	0	3/12/2013	3/06/2014	6/05/2014	4/11/2014	17/06/2013	Closed
03	21/06/2013		Delays associated with Conservation Mangement Plan Issues		12	12	19/12/2013	19/06/2014	22/05/2014	20/11/2014	21/06/2013	Closed
04	21/06/2013		Wet Weather 12-14 June 2013		1	1	20/12/2013	20/06/2014	23/05/2014	21/11/2014	21/06/2013	Closed
05	27/07/2013		Wet Weather 24 - 25 June 2013		2	2	7/01/2014	24/06/2014	27/05/2014	25/11/2014	27/06/2013	Closed
06	31/07/2013		Wet Weather 16-19 July 2013		4	4	13/01/2014	14/07/2014	2/06/2014	1/12/2014	31/07/2013	Closed
07	22/08/2013		Wet Weather 6th & 8th August 2013		2	2	15/01/2014	16/07/2014	4/06/2014	3/12/2014	27/08/2013	Closed
08	4/09/2013		Wet Weather, Fri 30 August 2013		1	1	16/01/2014	17/07/2014	5/06/2014	4/12/2014	16/09/2013	Closed
09	25/09/2013		Wet Weather - 16-20 September 2013		5	5	23/01/2014	24/07/2014	12/06/2014	11/12/2014	26/09/2013	Closed
10	27/11/2013		Wet Weather - 12-18 November 2013		5	5	3/02/2014	5/08/2014	19/06/2014	18/12/2014	27/11/2013	Closed
N/A			Amendment to HPD2 SP1&3 Dates for Practical Completion to coincide with completion of HPD1				28/03/2014	26/09/2014	28/03/2014	26/09/2014	17/12/2013	

EOT Number	Date Raised	Contract Issue Number (if applicable)	Description	Raised By	EOT Claimed (No of Days)	EOT Granted (No of Days)	Revised Date for Practical Completion				Date Hewatt Advised	Status
							SP1	SP2	SP3	SP4		
11	22/01/2014		Inclement Weather (High Temperatures) 15-17 Jan 2014		1.5	1.5	31/03/2014	29/09/2014	31/03/2014	29/09/2014	23/01/2014	Closed
12	6/02/2014		Inclement Weather (High Temperatures) 29 Jan - 3 Feb 2014		2	2	2/04/2014	7/10/2014	2/04/2014	7/10/2014	6/02/2014	Closed
13	24/02/2014		Wet Weather 19-20 February 2014		0.5	0.5	3/04/2014	7/10/2014	3/04/2014	7/10/2014	26/02/2014	Closed
14	6/03/2014		Wet Weather 5 March 2014		0.5	0.5	3/04/2014	7/10/2014	3/04/2014	7/10/2014	7/03/2014	Closed
15	31/03/2014		Delays due to redesign of pathways beneath bridge		5							Submitted
16	31/03/2014		Delays due to Wet Weather 24-31 March 2014		6							Submitted
				TOTALS	63.5	36.5						

Appendix C. Issue Register

HORSE PARK DRIVE EXTENSION – BURRUMARRA AVE TO MIRRABEI DR Contract No: 2013.20148.110

SMEC

ISSUES REGISTER - OPEN

Issue Number	Description	Details	Date Raised	Action	Status	Additional Comments
002	Issues surrounding Springbank Rise (Casey Development) Interface	There were a number of initial concerns from Lend Lease regarding the boundary interface with the HPD2 Project. 1. Locations of stockpiles 2. Casey Headwall	19/04/2013	The interface with Springbank Rise requires ongoing management. Current issues include ensuring HPD2 completion is well defined at interface.	Open	
018	Outcomes of SMA Investigations on HPD1	EDD engaged a consultant to prepare a report for the placement and use of SMA on HPD1. Outcomes of the report may influence the SMA to be used for HPD2.	7/02/2014	Hewatt have submitted a Variation for the changes to the SMA. Hold Points for Mix Designs have also been submitted.	Open	
019	Handover & WAE for Practical Completion	The requirements for handover of the completed project will be tracked as an issue. HPD2 handover will be completed through Asset Acceptance	7/02/2014	Hewatt has provided a draft schedule for handover and WAE submission.	Open	
020	Existing Telstra Pit to be raised at CH40	The existing Telstra Pit located at CH 40 requires raising by approximately 500mm to the FSL. This pit connects the HPD2 Telstra conduit to the existing network of conduits along Horse Park Dr. The works require an accredited Telstra subcontractor.	10/02/2014	Telstra has prepared a quote for approx. $ (excl GST) to raise the pit. Hewatt and Telstra are in correspondence to ensure Telstra has a subcontractor that is IRE certified.	Open	
022	Stone Pitching Finishes on Southern bridge embankment	EDD have raised issue with the finish of the stone pitching on the southern bridge abutment. Issues include protruding rock, waves in the finished levels and not in accordance with test panel.	15/11/2013	SMEC have issued a formal instruction to remove the stone pitching.	Open	
023	HPD1/2 Potential Opening Event	The ACT Government has shown interest in organising an Opening Event for the completion of HPD1 & HPD2.	7/03/2014		Open	

Monday, 28 April 2014

Appendix D. Variation Register

HORSE PARK DRIVE EXTENSION – BURRUMARRA AVE TO MIRRABEI DR
Contract No: 2013.20148.110

SMEC

VARIATIONS - REGISTER

Variation Number	Contract Issue Number	Description	Price Submitted (excl GST)	Price Agreed (excl GST)	Amount Certified to Date (excl GST)	Classification	Support Information	Status
		Quantity Overruns/Underruns						Agreed
01	003	Conservation Management Plan				Design/ Document deficiency		Closed
02	003	Delays associated with Conservation Management Plan				Latent/ Site Condition		Closed
03		Not used				Not used		
04	003	Site Visit by Heritage Consultant to advise on CMP				Design/ Document deficiency		Closed
05	002	Regrading of mounds between HPD2 & Springbank Rise				Latent/ Site Condition		Closed
06	N/A	Changes to the Watermain from Tender				Scope Change		Closed
07	N/A	Supply and Installation of Combined Usage Pole				Contractor Proposed Alternative		Agreed
08	N/A	Select CBR Changes from Design to Construction				Design/ Document deficiency		Closed
09	008	Pricing of Missing Pay Items from the Bill of Quantities				Design/ Document deficiency		Agreed
10	N/A	Supply of 375mm uPVC Sewer Pipe not used				Design Improvement		Closed
11	N/A	Anti-graffiti coating on bridge surfaces				Superintendent Request		Agreed
12	007	Survey Works for Fence Realignment at Swale 15				Design/ Document deficiency		Closed
13	N/A	Additional Works for the Floodway				Scope Change		Closed
16		Not used						
17	002	Additional Works associated with Casey Headwall (excl stone pitching)				Latent/ Site Condition		Agreed
18	017	Additional works with Gas Main from Ch60 - Ch20				Latent/ Site Condition		Closed
19		Widening of mowing strip along PD1 fence alignment						Agreed

Variation Number	Contract Issue Number	Description	Price Submitted (excl GST)	Price Agreed (excl GST)	Amount Certified to Date (excl GST)	Classification	Support Information	Status
21	017	Additional works with Telstra from Ch20 - Ch60				Latent/ Site Condition		Closed
22	N/A	Additional Length of PD1 Fencing with In-situ Footings				Limit of Accuracy		Closed
23	N/A	Additional works to provide drainage to subsoils and carriageway at the end of Mirrabei Dr				Design/ Document deficiency		Closed
24		Not used						
25	011	Additional Shared Trench Road Crossings at CH970 and CH1500				Design Improvement		Closed
26	N/A	Deduction for the use of Jute Mesh as opposed to Jute Mat				Contractor Proposed Alternative		Agreed
27	N/A	Civil works associated with providing Gas main stub to Ngunnawal as required by ZNX/Jemena				Design/ Document deficiency		Closed
29	013	HPD1 & HPD2 Quantity Adjustments for Changes in Scope of Works				Scope Change		Pending
30	N/A	Additional Shared Trench at East side of Mirrabei Dr				Superintendent Request		Closed
31	N/A	Additional civil & electrical works for electrical supply to streetlight and traffic light controllers (excl ActewAGL work)				Operational		Awaiting further info from Hewatt
34		Deduction for use of SL82 instead of SL81 in islands				Design Improvement		Agreed
36	020	Raising Telstra Pit to Design Level at CH40				Latent/ Site Condition	Yes	To be claimed under 815P2
38	018	Change to SMA10 at 30mm and use of RMS R121				Design Improvement		Pending
		TOTALS						

Appendix E. Estimated Final Contract Value Summary

Horse Park Drive Extension Project (Burrumarra Ave to Mirrabei Dr) - March 2014

ESTIMATED FINAL CONTRACT VALUE

Principal: ACT Shared Services Procurement
Superintendent: SMEC Australia Pty Ltd
Contractor: Hewatt Pty Ltd
Contract No: 2013.21048.110

Period Up To	end of Mar-2014

Section No	Description	Original Contract Value (excl GST)	Estimated Final Value (excl GST)	Difference	Quantity Overruns to Date
Civil Works					
0	Preliminaries	$			
1	Provision for Traffic	$			
2	Earthworks	$			
3	Underground Services	$			
	Excavation and Backfilling	$			
	Sewerage	$			
	Stormwater Drainage	$			
	Subsoil Drains	$			
	Conduits	$			
	Water Supply Mains	$			
4	Flexible Pavement Construction	$			
6	Concrete Kerbs, Footpath & Minor works	$			
7	Road Furniture	$			
8	Incidental works	$			
9	Landscape	$			
10	Road Signs	$			
11	Pavement Marking	$			
13	Traffic Signals	$			
14	Street Lighting	$			
16	Bridge Works	$			
	Subtotal:	$			
Variations		A			
	Subtotal:	$			
Quantity Overruns/Underruns					

Total (Excl GST): $

GST Applicable: $

Total (Incl GST): $

Appendix F. RFI Register

HORSE PARK DRIVE EXTENSION – BURR. .IARRA AVE TO MIRRABEI DR C act No: 2013.20148.110

REQUEST FOR INFORMATION (RFI) - OPEN OR THIS MONTH

SMEC

Status Legend:

- Not Urgent
- Moderate Priority (1-2 weeks)
- Urgent (1-2 days)

RFI Number	Description	Date Raised	Issued to Designer?	Date Issued to Brown	Response From Brown	Discipline:	Classification	Comment	Response Issued to Contractor	Status
116A	Clarification of sign post locations	4/03/2014	No			Road Furniture	Design Clarification		4/03/2014	Closed
117A	Clarification of crossfalls provided as part of response to RFI109A	6/03/2014	Yes	6/03/2014	13/03/2014	Concrete Kerbs, Footpaths and Minor Works	Design Clarification		14/03/2014	Closed
119A	Clarification of backfill requirements for tree planting	14/03/2014	Yes	14/03/2014	14/03/2014	Landscaping	Specification Clarification		14/03/2014	Closed
120A	Clarification of asphalt joints (Bridge site & tie-in to Burrumarra Ave)	28/03/2014				Pavement	Design Clarification		3/04/2014	Closed

Total = 4

Appendix G. Public Liaison Register

PUBLIC LIAISON REGISTER - ALL

SMEC

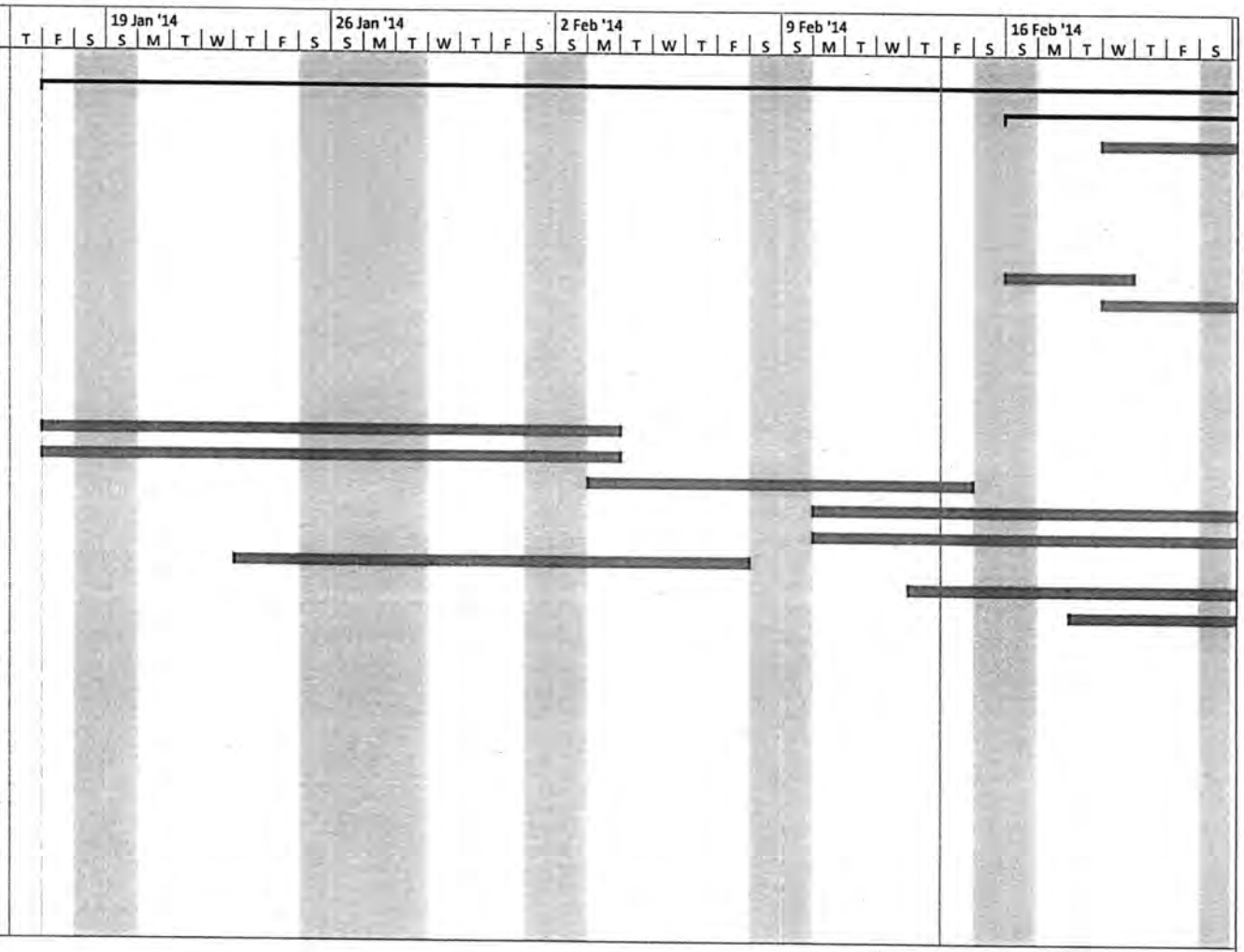
Liaison No.	Date Raised	Liaison Initiator	Person Contacted	Location	Description	Date Actioned	Action Taken	Status	Comment
1	29/04/2013	(SMEC)	(BN1)	Site - General	Numerous discussions have been held with regarding the re-alignment of the Bicentennial National Trail throughout construction.	9/05/2013	Following meeting with on-site and agreeing on a re-alignment, signage previously within HPD2 alignment was relocated along the new alignment	Closed	
2	2/05/2013			CH0-450	Project Interface Meeting held at SMEC Offices to discuss potential interface issues between the Lend Lease Casey Development and HPD2	2/05/2013	Minutes of the meeting were taken and actions assigned to each party. This action will be ongoing for the most part of SP1.	Closed	
3	29/05/2013				Hewatt contacted regarding access within his property to complete the designed works.		Ongoing meetings are being held with	Closed	Refer to ISS007
4	13/06/2013			Telephone	Organised a meeting on 14/06/2013 to discuss fencing works associated with the Project on his property. also advised he wished to move his sheep/cattle from	14/06/2013	Meeting scheduled for 14/06/2013	Closed	
5	14/06/2013	Site (Hewatt-SMEC)		Site - next to	Meeting held with : The farmer has asked to drive his stock across the site at the end of July; Fencing location was agreed as well as the installation of an additional gate.	26/06/2013	Gate installed and rules put in place to allow to relocate his cattle and sheep.	Closed	
6	12/06/2013		John Husband	Email	I contacted the ACT Government to enquire about the timing of the Horse Park Drive Projects	18/06/2013	John Husband replied to advising the estimated completion dates for the two projects.	Closed	
7	12/07/2013	r,John Husband		On Site at	and John met with to discuss works designed within and various approvals that may be required to conduct these works (including land withdrawal).	16/07/2013	Hewatt to set up pegs along intended boundary alignment; survey to be undertaken and ACTPLA to be contacted to initiate dicussions. Another meeting to be held with soon.	Closed	
8	12/07/2013			Email	I was asking about the timing for the completion of the HPD works.	16/07/2013	advised I of the likely completion dates.	Closed	
9	16/07/2013	Rise)		On site -	and met with to show him the marked out pegs and to discuss locations of gates, etc.	5/08/2013	Ongoing actions: 1. Get survey plan to show ACTPLA; 2. Initiate withdrawal; 3. Attempt to set up	Closed	
10	12/07/2013	Member of Public	Hewatt Contractors	Hewatt Site Office	A member of the public called into the site office to ask about the expected completion date of the Project.	12/07/2013	Hewatt advised the person it would likely be completed by mid-2014	Closed	

Liaison No.	Date Raised	Liaison Initiator	Person Contacted	Location	Description	Date Actioned	Action Taken	Status	Comment
11	5/08/2013	John Husband			Meeting with to discuss potential to access property prior to land withdrawal. did not agree as he is in the process of selling the property	5/08/2013	Hewatt were informed of the situation and alternative arrangements are to be made.	Closed	
12	7/08/2013		Patrick Hancock/John Husband	Email	I was wondering the staging of the two remaining HPD Projects and possible implications to traffic through Ngunnawal.	7/08/2013	Patrick informed the Jacka/Amaroo end will be completed 1st and then Stage 2 (Casey end).	Closed	
13	12/08/2013		T. Gordon (ACT Government)	Meeting Venue	T. Gordon attaended the / I meeting within the week commencing 12/8/13 to update the meeting on HPD1/HPD2 progress.		N/A	Closed	
14	8/08/2013	Member of the Public	Chief Minister Talk Back	N/A	A member of the Public asked a question during the Chief Minister Talk Back time about the timing of the HPD1 and HPD2 Project completion dates.	9/08/2013	A response was due to be issued on Fri 9/8/13.	Closed	
15	16/08/2013			Phone	contacted regarding works required within (which he has a 'grazing license' on). I was happy for works to go ahead as long as he was notified and fences remained stockproof.	28/10/2013	Informed Refer to Liaison No. 20	Closed	
16	19/09/2013			Phone	contacted regarding the sale of his land. He advised that settlement is still due on the 30th Sep 2013 and that should call after this date and details of the new owner can be exchanged.	4/10/2013	contacted on the 4th Oct 2013. gave the contact details of the new owner and informed that the sale was likely to settle on the 4th Oct 2013.	Closed	
17	1/10/2013	(Twitter)	ACT Govt	Twitter	was enquiring about completion dates for HPD1 and 2.	2/10/2013	John Husband responded to the appropriate people within ACT Govt, to then give an answer	Closed	
18	4/10/2013			Phone	contacted (new leaseholder of to initiate agreements about accessing his property for the construction of Swale 15 prior to the official land withdrawal. I was understanding and signed an agreement that was emailed to him a	10/10/2013	will keep informed of any Project related activities that will likely affect his property.	Closed	
19	14/10/2013			Phone	contacted (who has his cattle on agistment in to inform him that fencing works for the land withdrawal area were commencing.	14/10/2013	Hewatt completed fencing works surrounding and have isolated the construction area.	Closed	
20	28/10/2013			Phone	contacted to inform him of works within (on which he holds a grazing licence).	28/10/2013	Hewatt are completing works within the property. relocated livestock to another paddock whilst works are being completed	Open	
21	13/11/2013	Sharon Harmer (ACTPLA)		Letter	The formal notice for the withdrawal of land from was sent to and . The date of withdrawal will be 4 February 2014.	13/11/2013	N/A	Closed	
22	16/12/2013			Phone	called up about a few problems with the fencing along the eastern side of HPD where he has a grazing licence on LDA land. There were 3 locations that needed fixing.	18/12/2013	Hewatt repaired the fences such that they were stockproof for the Christmas shutdown period. returned phone call to inform him that the fences had been fixed (a more permanent fix will be completed in 2014).	Closed	

Liaison No.	Date Raised	Liaison Initiator	Person Contacted	Location	Description	Date Actioned	Action Taken	Status	Comment
23	16/01/2014	Hewatt		Phone	Hewatt informed that trenching works within Springbank Rise would commence on Monday 20 Jan 2014. did not have any issues with this.	31/01/2014	None required. will be informed when works are complete.	Closed	
24	30/01/2014			Email	was asking about the likely completion date for Hewatt's trenching works within Springbank Rise	31/01/2014	responded to , informing them that the trenching work would be completed within Springbank Rise by mid-week commencing 3/2/14	Closed	
25	13/02/2014			Email	, contacted to inform him that the Burrumarra Ave asphalt wearing course works would not be undertaken by the HPD2 Project. Therefore there wouldn't be TTMPs disturbing the entrance of Springbank Rise		N/A	Closed	
26	17/02/2014			Email	was wondering when the stockpiles were going to be bitumen straw mulched as he was organsing a TaMS inspection and the stockpiles would form part of this.	18/02/2014	has been in contact with to co-ordinate these works. These works are exclusive of HPD2.	Open	
27	20/02/2014	Cyclist		Burrumarra Ave	A cyclist was looking for the Bicentennial National Trail and his GPS was directing him into the site. & directed him through Ngunnawal to get to Mirrabei Dr.	20/02/2014	& directed him through Ngunnawal to get to Mirrabei Dr.	Closed	
28	12/03/2014	Member of Public	Canberra Connect	Telephone	Resident was wanting to know the completion date for the Horse Park Dr Extension.	17/03/2014	John Husband advised the caller HPDE will be completed in April 2014.	Closed	
29	24/03/2014			Springbank Rise	Hewatt and SMEC met with to discuss handover issues associated with HPD2 and SBR Interface.	24/03/2014	N/A	Closed	

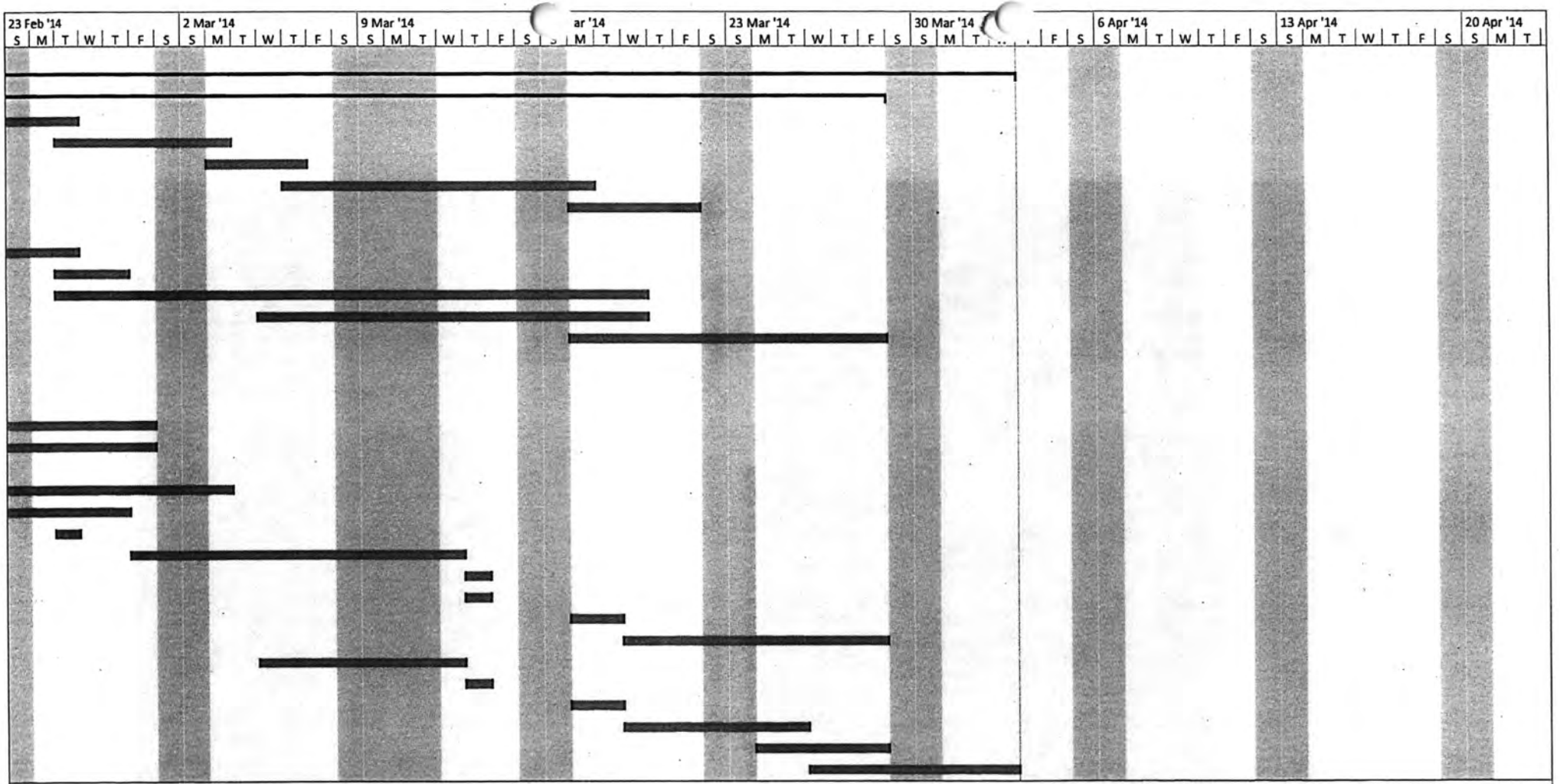
Appendix H. Program

ID	Task Mode	Task Name	Duration	Start	Finish
1					
2		**Horse Park Drive 2**	**50 days**	**Fri 17/01/14**	**Wed 2/04/14**
3		**Bridge Works**	**28 days**	**Sun 16/02/14**	**Fri 28/03/14**
4		Guardrail	5 days	Wed 19/02/14	Tue 25/02/14
5		Relieving slabs	5 days	Tue 25/02/14	Mon 3/03/14
6		Mowing Strip	4 days	Mon 3/03/14	Thu 6/03/14
7		Red Granite	6 days	Thu 6/03/14	Mon 17/03/14
8		Bridge Railings	5 days	Mon 17/03/14	Fri 21/03/14
9		Stormwater	4 days	Sun 16/02/14	Wed 19/02/14
10		Retaining wall	5 days	Wed 19/02/14	Tue 25/02/14
11		Kerbs under bri	3 days	Tue 25/02/14	Thu 27/02/14
12		Stone Pitching	15 days	Tue 25/02/14	Wed 19/03/14
13		Concrete footp	9 days	Wed 5/03/14	Wed 19/03/14
14		PD1 Handrail	10 days	Mon 17/03/14	Fri 28/03/14
15		Common Trench	10 days	Fri 17/01/14	Mon 3/02/14
16		Subsoil Drains	10 days	Fri 17/01/14	Mon 3/02/14
17		Streetlight condui	10 days	Mon 3/02/14	Fri 14/02/14
18		Streetlight Colum	15 days	Mon 10/02/14	Fri 28/02/14
19		Traffic Lights	15 days	Mon 10/02/14	Fri 28/02/14
20		Subbase	10 days	Thu 23/01/14	Fri 7/02/14
21		Kerbs / Islands	13 days	Thu 13/02/14	Mon 3/03/14
22		Island Infills	8 days	Tue 18/02/14	Thu 27/02/14
23		Bridge Epoxy	1 day	Tue 25/02/14	Tue 25/02/14
24		Base	7 days	Fri 28/02/14	Wed 12/03/14
25		Prime	1 day	Thu 13/03/14	Thu 13/03/14
26		SAMI Seal - Bridge	1 day	Thu 13/03/14	Thu 13/03/14
27		14/7 Seal	2 days	Mon 17/03/14	Tue 18/03/14
28		AC SMA	8 days	Wed 19/03/14	Fri 28/03/14
29		Cyclepath base	4 days	Wed 5/03/14	Wed 12/03/14
30		Cyclepath prime	1 day	Thu 13/03/14	Thu 13/03/14
31		AC FGG	2 days	Mon 17/03/14	Tue 18/03/14
32		Topsoil	5 days	Wed 19/03/14	Tue 25/03/14
33		Planting	5 days	Mon 24/03/14	Fri 28/03/14
34		Lines/Signs/Rails	6 days	Wed 26/03/14	Wed 2/04/14



Project: Horse Park Dr 2
Date: Fri 14/02/14

Task		Project Summary		Inactive Milestone		Manual Summary Rollup		Deadline	
Split		External Tasks		Inactive Summary		Manual Summary		Progress	
Milestone		External Milestone		Manual Task		Start-only		Manual Progress	
Summary		Inactive Task		Duration-only		Finish-only			



Project: Horse Park Dr 2
Date: Fri 14/02/14

Task		Project Summary		Inactive Milestone		Manual Summary Rollup		Deadline	
Split		External Tasks		Inactive Summary		Manual Summary		Progress	
Milestone		External Milestone		Manual Task		Start-only		Manual Progress	
Summary		Inactive Task		Duration-only		Finish-only			

Appendix I. Photograph

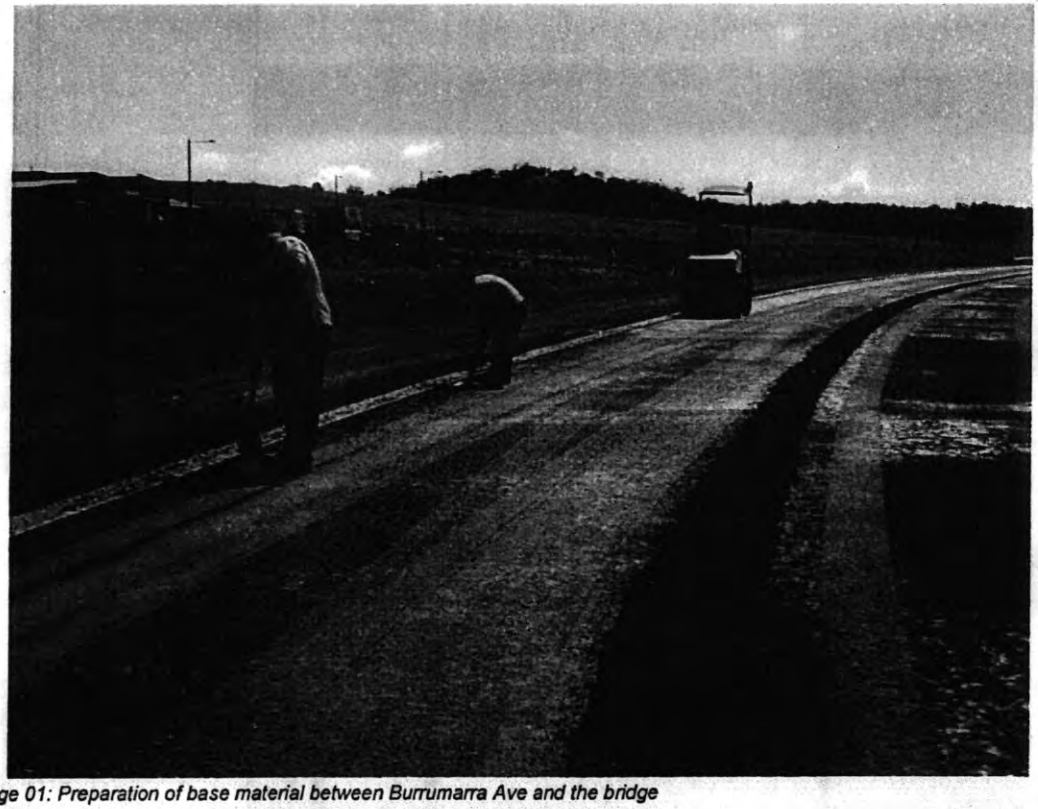


Image 01: Preparation of base material between Burrumarra Ave and the bridge

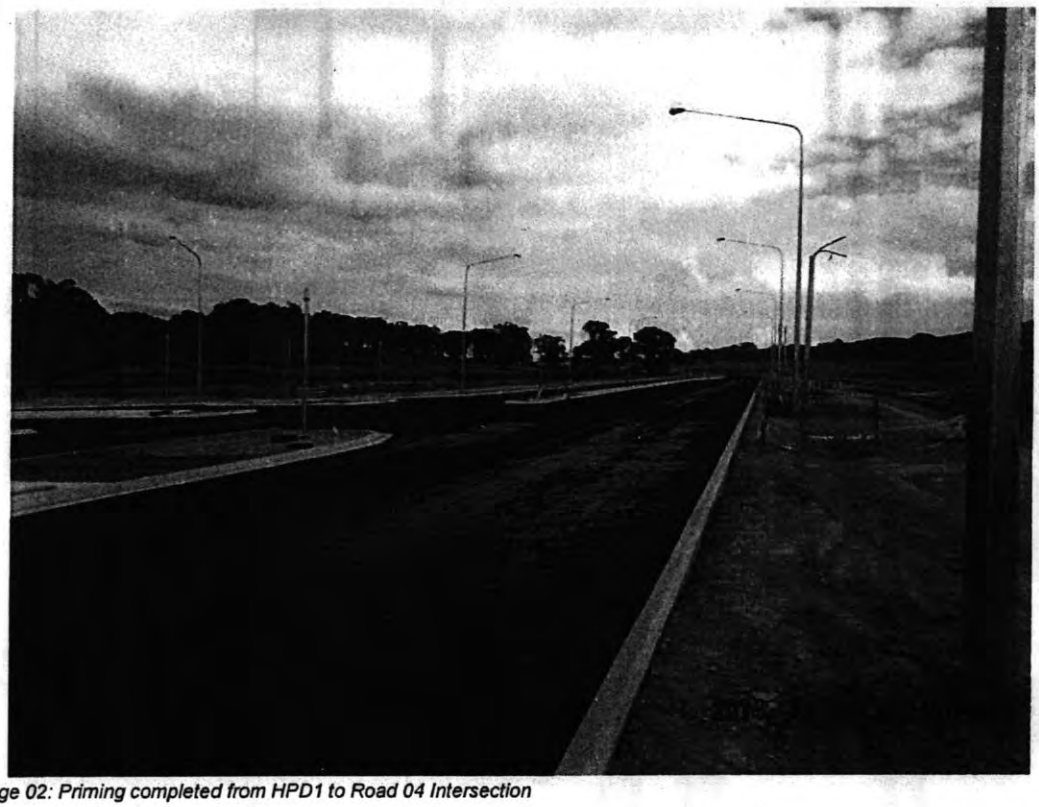


Image 02: Priming completed from HPD1 to Road 04 Intersection

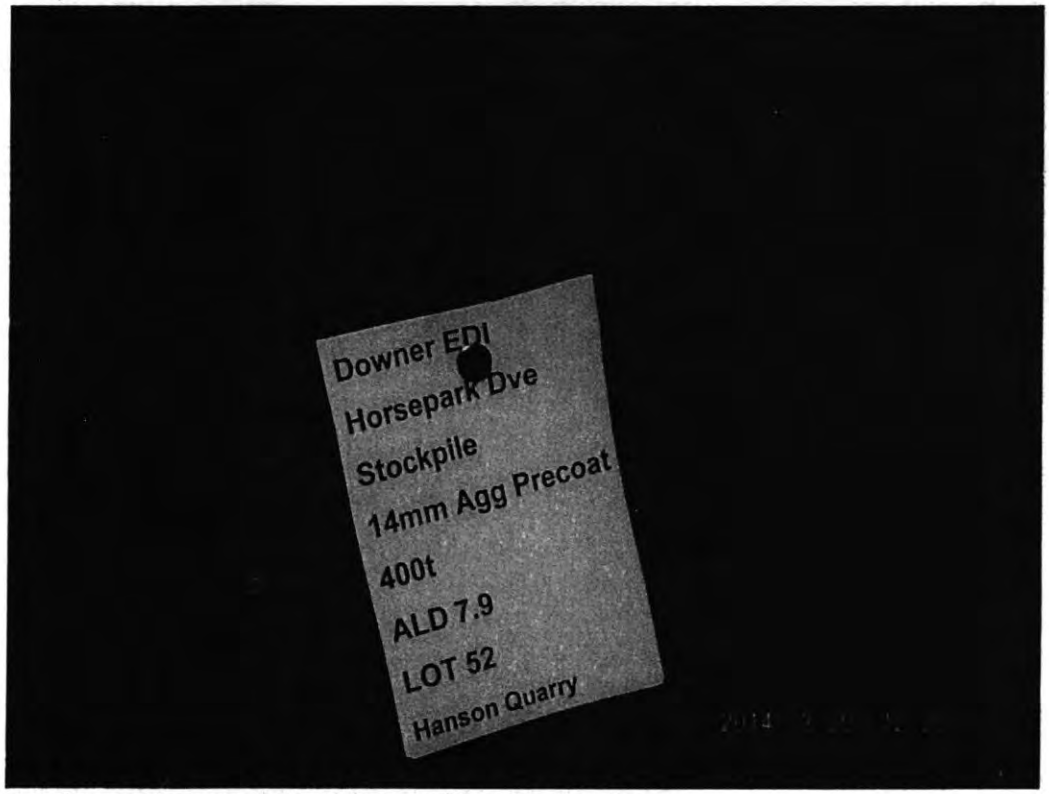


Image 03: Pre-coated aggregate for the 14/7mm seal delivered to site by Hanson

Image 04: Construction of stone pitched retaining wall beside floodway

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

WEEKLY REPORT FOR WEEK ENDING: 22 March 2014

1. **WHS**
   a. Site inductions continuing - 206 personnel to date;
   b. WHS audit of the project conducted by I from Kaizen Management Services.

2. **Traffic Management**
   a. No changes – site vehicles continuing to use main carriageway for access throughout Project (shoulders being utilised during priming and sealing works).

3. **Design**
   a. *Confirmation of changes to pavement design (RFI112A)* – Hewatt has raised a number of queries regarding the use of RMS Specification R121. SMEC have passed these onto EDD (with comments) for their response.

4. **Construction**
   a. Placement of DGB base layer completed;
   b. Spraying of prime completed from CH880 to CH1850 (interface with existing seal at HPD1);
   c. Cabling of power supply to the street lights continuing;
   d. Shared Path – placement of DGB material continuing;
   e. Construction of equestrian/shared path retaining wall continuing;
   f. Concrete footpath sections completed at the bridge;
   g. Pedestrian handrail installation continuing at the bridge, footings completed and remaining panels to be installed;
   h. Preparation of base and mowing strips for decomposed granite under the guard rail at the bridge approaches continuing;
   i. Concrete infill in traffic islands continuing;
   j. Pram ramps in traffic islands completed;
   k. Concrete base and associated conduits for Traffic Control Unit installed;
   l. Topsoiling and landscape activities continuing.

5. **Meetings**
   a. Hewatt, SMEC, SSP, EDD and Roads ACT met on-site on Tuesday 18 March 2014 to inspect and discuss stone-pitching issues. Roads ACT considered the stone pitching to be unacceptable due to its inconsistent finish;
   b. Fortnightly Site Meeting held on Friday 21st March 2014.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Hewatt will continue to correspond with Telstra to identify a suitable contractor to complete the works;
   b. *Excess material potentially requiring disposal* – it appears there may be a small amount of material that will not be able to be disposed on-site (<1000m3). This is to be confirmed as finished surface levels are finalised;
   c. *Interface with Springbank Rise (ISS002)* – Hewatt/SMEC will meet with Lend Lease on Monday 24 March 2014;
   d. *Stone-pitching finishes (ISS022)* – Hewatt have responded to SMEC's request for conformance information relating to the stone pitching. Hewatt's interpretation of Clause 8.13.1 differs to the Principal's interpretation.

7. Payments

a. Hewatt has submitted Tax Invoice No. 7514 for for Payment Claim No. 11 - February 2014;
b. Brown are currently completing a review of Imported Fill and General Earthwork Quantities to be used in SMEC's assessment of final earthwork quantities;
c. SMEC and Hewatt will work together to have Hewatt's Tax Invoice for Payment Claim No. 12 – March 2014 submitted by Friday 11 April 2014.

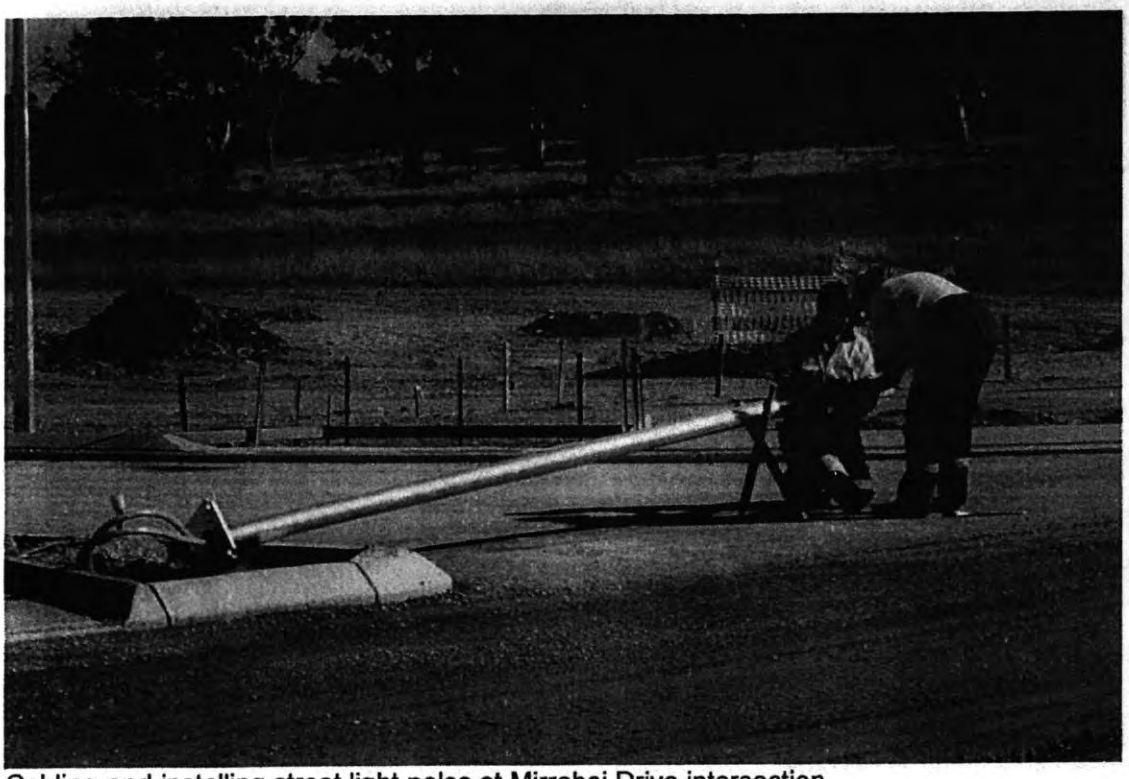
8. Activities Planned for the Upcoming Fortnight

Forecast inclement weather for the commencing 24th March will cause construction delays. Remaining section of prime placement CH50 to CH880 planned for Monday 24 February had to be cancelled.

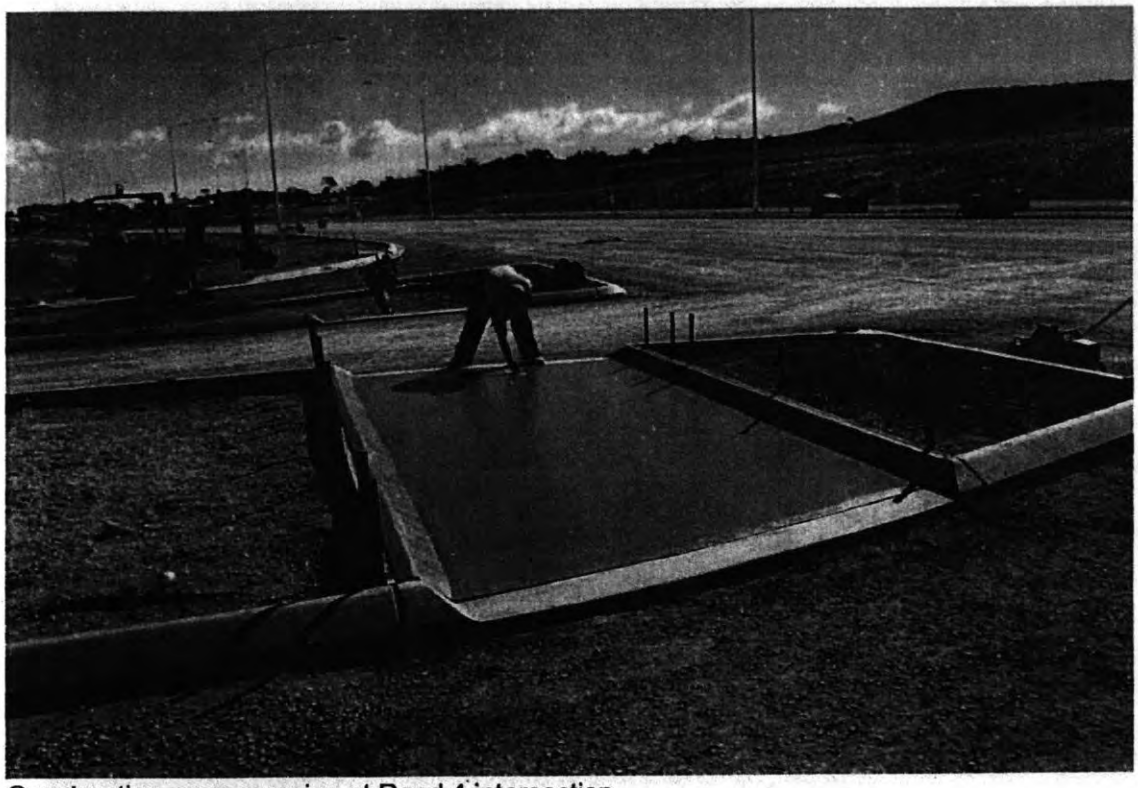
a. Place prime on the remaining section completed base CH50 to CH880;
b. Commence placement of 14/7mm two coat seal;
c. Complete footing for retaining wall between shared and equestrian paths under the bridge and continue construction of the wall;
d. Landscape activities to continue across the site;
e. Connection of power supply from ActewAGL's mini-pillars to the streetlights and traffic lights;
f. Continue trimming of DGB to design levels for the base of the shared path;
g. Construct flushing points for sub soil high end and intermediate risers;
h. Complete mowing strips for decomposed granite area under the guard rails at the bridge approaches;
i. Continue with concrete infill of traffic islands.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos



Cabling and installing street light poles at Mirrabei Drive intersection



Constructing pram crossing at Road 4 intersection

Application of prime at CH1700

Placement of base for shared path at CH250

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

MEETING: SITE MEETING - MINUTES

LOCATION: Project Site Office, Horse Park Drive, Jacka
DATE: Friday, 21 March 2014
TIME: 10.30am
ATTENDEES:

Economic Development Directorate: *Michael McGrath (MM)*
John Husband (JH)
Michael Sergi (MS)

Shared Services Procurement: Patrick Hancock (PH)
James Cargill (JC)

Brown:

SMEC:

Hewatt:

No.	Item Description	Action
01.	WHS • ACT Govt. WHS Audit was held on Monday 17 March 2014 by (Kaizen). Feedback was very positive from a project perspective (an unofficial 9/10 was given for the Project by). raised a number of recommendations for SMEC, which SMEC will follow up with. Hewatt and SMEC will await the follow up report prepared by Kaizen. • confirmed that the incident involving the guardrail post driving machine and operator was not a 'notifiable incident' as the worker was discharged immediately from hospital and there were less than 7 days LTI recorded. noted that the worker returned to work the next week after 3 days LTI. • informed the meeting that Hewatt's WHS Manager conducted an internal WHS Audit on Thurs. 13/3/14. A couple of NCRs and a number of observations were raised.	Kaizen/SSP
02.	Review Previous Minutes / Matters Arising • Hewatt submitted Variation Claim No. 31 relating to the additional works carried out associated with the power supply to the streetlight and traffic light controllers. See Item 14. • It was agreed to use 17 April 2014 as an approx. completion date for purposes of advising EDD Communications section and a potential opening event. has raised this item as an issue to ensure relevant parties are kept up-to-date. will act as the liaison in keeping EDD Communications informed.	JH
03.	Design • RFIs: 116 submitted; 0 open • issued response to RFI117A, providing revised and conforming crossfalls for the merger of the trunk path and concrete path adjacent to the floodway. • SMA Specification Changes – JH noted EDD had received queries (& comments) on the use of RMS Specification R121. JH indicated that EDD considers a PQP necessary and is considering the use of the 'Incentives' clause for ride quality. JH	

	will respond to this email shortly. o noted that the ride quality on HPD1 was not great and there may be deductions associated with this. Hewatt are expecting the ride quality on HPD2 to be considerably better. The 'incentives' clause provides a measurable way of balancing out the potential deductions associated with ride quality. JH was to liaise with MM and consider this. o also raised the issues associated with the second half of Clause 1.1 (R121) which refers to a 24 month period within which no rutting, shoving, bleeding, etc. must occur. This requirement is outside the project's 12 month DLP and the specification also suggests SMA should not be used in intersections. suggested in comments to JH that the 24 month period could be revised to 12 months and this clause could be relaxed for intersection areas. JH will consider these points. o also commented that in his opinion the intersections on HPD2 may fail within the 24 months due to the high traffic loading and the use of Mirrabei Dr and Road 04 as construction access points to Moncrieff subdivision works. • Hewatt have submitted a Hold Point for the SMA10 mix design. is reviewing. noted under R121, this Hold Point only needs to be submitted 7 working days prior to placing SMA. • is reviewing the Prime and Seal Design. The proposed program for prime, seal and SMA works are shown in Item 05. noted that the prime is a 'light' prime, using AMC00 as per the design. confirmed that the seal binder is to be a straight C170 binder (ie. 0% cutter) and the standard 14/7mm double-double seal. Given the use of no cutter, the binder only needs time to cool prior to SMA placement (as no volatiles need to bleed off). • sent note through to MS & JH regarding hazard free zone requirement behind guardrails. • JH will respond to an LDA query relating to Bus Stops on HPD1&2 (construction had progressed too far when ACTION requested that bus stops be included. Any bus stops will have to be retrofitted, following the completion of HPD2). • asked if Roads ACT had a time period in mind to complete the wearing course works around Burrumarra Ave roundabout. PH noted no time frame had been given, just a note that Roads ACT will complete these works. noted that the response to RFI038A (widened carriageway between CH60-90) included the recommendation to use temporary kerbing beside the carriageway. It was agreed this requirement could be reviewed following completion of SMA. to raise this at this point. • also asked whether the concrete path along Whitrod Ave that terminates at HPD2 limit of works would be extended as part of HPD2. (It was originally included in the proposal to complete the additional wearing course works). PH said to leave it out of the HPD2 scope of works, if Roads ACT wish to extend they can. would send PH a photo of the location regardless.	JH JH
04.	Site Compound • The aim is to have to have the site compound removed by Easter (17 April 2014). The site will be landscaped following	

	removal of sheds.	
05.	Program • outlined program for prime, seal and SMA works (as shown below). SMA works will commence in HPD1 first. There is the potential for two asphalt crews to be working at once (1 in HPD1 and 1 in HPD2).	

17/03/2014	18/03/2014	19/03/2014	20/03/2014	21/03/2014	22/03/2014	23/03/2014
Mon	Tues	Wed	Thu	Fri	Sat	Sun
				Prime		
24/03/2014	25/03/2014	26/03/2014	27/03/2014	28/03/2014	29/03/2014	30/03/2014
Mon	Tues	Wed	Thu	Fri	Sat	Sun
Prime						
31/03/2014	1/04/2014	2/04/2014	3/04/2014	4/04/2014	5/04/2014	6/04/2014
Mon	Tues	Wed	Thu	Fri	Sat	Sun
SMA (HPD1 & HPD2) - Approximately 8 days for HPD2, 2-3 for HPD1						

Current PC Dates		
SP1 & SP3	Civil, Burrumarra Ave – HPD1	03 Apr 2014
SP2 & SP4	Landscape, Burrumarra Ave – HPD1	07 Oct 2014

06.	Environmental • Jill Harrop (ACT EPA) will visit the site on Tues 25 Mar 2014. Visit will review if Hewatt's works have been carried out in accordance with the waterway licence. • has organised a meeting with t (Lend Lease) next week. Silt in the floodway and LL's environmental controls will be an agenda item.	
07.	Industrial Relations • **Raising Telstra Pit at CH40**: Hewatt planned to forward an email from ACT Govt through to Telstra, showing there will be no leniency given to Telstra's subcontractors who do not have ACT IRE Certification. asked if PH could send an email and will forward to Hewatt. o The intended action is to isolate the area surrounding the pit and continue works either side, until this issue can be resolved.	PH/
08.	Traffic • PH sent email to B. McHugh asking how Roads ACT would like HPD2 to leave the Traffic Lights at Mirrabei Dr. PH has not received a reply. • Moncrieff West subdivision will be out for tender very shortly. It is likely the traffic lights at Mirrabei Dr will be required by June 2014 for construction access. It was agreed by meeting incl. JH & PH that works/proceedings will be carried out to have the lights as operational at Project completion. If there are delays to Moncrieff for any reason and they are not required, Roads ACT can simply disable them or change the phasing.	
09.	Community • added community consultation through Canberra Connect to the PLR. • A meeting has been organised with Lend Lease to discuss any issues with HPD2-Springbank Rise interface. • also intends to organise a meeting with Ngunnawal 2C (Superintendents or Contractors) to confirm project boundaries and to ensure landscaping is consistent at the interface.	
10.	Staffing • (Foreman/Supervisor) will stay on with HPD2 until 1st week of April 2014.	

	• Hewatt had a number of staff changes throughout the week. The largest of which (for HPD2 purposes) was i finishing up on the Project. was the Snr Project Engineer. will be taking over from and will be spending a significant proportion of his time on-site.	
11.	Utilities • ZNX to connect HPD2 gas to existing at Burrumarra Ave and HPD1 (planned for Tues 25/03). All excavations around the gas line should be complete. noted that the Telstra Pit at CH40 is on the other side of the NBN Pit to the gas – therefore any works for this should not be near the live gas. • ActewAGL electrical connection will occur following Ecowise completion of works on the TCU and SLC. • SMEC will complete stormwater inspection for HPD2 for handover. • MS noted that the Stormwater Inspection Contract had been extended to include roads (as well as subdivisions). • will issue updated hydraulics WAE to SMEC for submission to Actew Water. • All WAE information has been recorded for the shared utilities trench – it still needs to be compiled and put into drawings. NBN Co. is awaiting this information prior to completing a handover inspection. • & asked who should be contacted regarding the data connection for the Telstra line to the TCU. MS indicated that this is something that TaMS (Cameron Glassford) organise when the traffic lights are linked up. • MS noted on a walk-through with TaMS on another project, TaMS picked up a number of issues with street-lighting (including the requirement that the bottom of the access panel is 450-500mm from ground level).	Hewatt

12.	Issues
	• The stone-pitching is an ongoing issue. noted that Hewatt will continue to complete works for stone pitching. Comments on quality have been noted by Hewatt and future stone pitching will take this into account.
	• Handover and WAE for PC (ISS019):
	  o to contact Asset Acceptance to seek clarification on disposition of cracked kerbs;
	  o SMEC will issue a draft defects register early next week (week commencing 24/3/14).
	• A meeting will be held with Lend Lease regarding interface issues with Springbank Rise.
	• also hopes to meet with Canberra Contractors/Brown to discuss interface with N2C. noted that project boundaries have been agreed in email correspondence.
		SMEC

13.	EOT • EOT Status (14 submitted): 52.5 Days Claimed; 36.5 Days Granted • asked whether the date for PC was going to be strictly enforced. noted that the current completion date is expected to be a few days after the current date for PC and asked whether this was going to be seen as an issue. The original revised date for PC of 28 March 2014 was proposed by as part of the proposal to merge remaining portions of HPD1 and	

	HPD2. o noted that if the date for PC was going to be strictly enforced, there is the potential for delay claims due to a number of design changes, as well as additional EOTs for wet weather. o PH/JH noted request to start discussions was valid. JH would discuss with MM. will send through a short list of potential delays to be considered by JH/MM in decision making.	JH/
14.	Variations • **Status** (including BoQ over/underruns) (excl GST): Submitted; Endorsed; Certified for payment (as at end of Feb '14) • submitted by with February 2014 Claim. A number of items were missing and asked SS to revise. will forward a quick email to with comments. Hewatt to resubmit. has partly assessed the claim and certified payment for the part that was assessed. • Hewatt have submitted a negative variation for the change to SMA10 with 30mm nominal thickness. to make an assessment and forward to & SSP/EDD. • to be claimed under the Telstra Provisional Sum Item 815P2.	
15.	Progress Claims • to revise Cash Flow forecasts and send to SMEC. • PH noted the requirements for March 2014 Tax Invoices (from both Consultants and Contractors) to be submitted early due to SSP Finance System being upgraded. Dates for submissions are within email (sent to SMEC and Hewatt). Pat needs it at the very latest by 16 April 2014 (preferably beforehand). (**Post-meeting note:** PH's email asked for Contractor's Invoices in by Fri 11 April). o SMEC and Hewatt will work together to have the claim & Hewatt's Tax Invoice in within this time frame. • JH asked if the financials are being divided between HPD1 and HPD2 (for internal EDD financing purposes). noted that this issue had been resolved at a project level with a Variation raised on both HPD1 & 2 for any adjustments. PH noted that SSP will be able to divide the financial reporting for HPD1/2 as required for EDD's purposes. • Payment for Hewatt's February 2014 Tax Invoice is for Thurs 27/3/14.	
16.	Quality Assurance • HPs: 302 raised; 18 open • There are a number of outstanding Hold Points requiring additional information from Hewatt. is following these up ASAP. • has forwarded email to Downer regarding Prime and Seal HP to obtain the necessary information. • The SMA Hold Point should be resubmitted under RMS R121 Specification Requirements. • NCRs: 30 raised (25 Quality; 5 Safety) • Hewatt raised an NCR regarding the testing procedures carried out by J&A Testing. Hewatt/SMEC will await a response from J&A prior to taking the matter further.	Hewatt

17.	Working hours and days • *(Note copied across from last meeting's minutes)* Hewatt's Easter Shutdown (for other projects) will be from 18 – 27 April 2014 (inclusive). It is anticipated that HPD2 will be completed by this stage, however if not work may continue on non-Public Holiday days in order to complete the Project (RDOs will be banked for later if this is the case).	
18.	Construction Matters • JH will follow up on the treatments to the end of the road stubs. • will prepare a report on the pavement construction in the road stubs (this will include photos and measurements of the pavement constructed).	JH SMEC
19.	Subcontractors • will finalise contract documents for Downer and forward these through to SMEC.	
20.	Next Meeting(s) • Friday, 4 April 2014 – Site Meeting	
21.	Other?	

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

WEEKLY REPORT FOR WEEK ENDING: 15 March 2014

1. **WHS**
   a. Site inductions continuing - 206 personnel to date

2. **Traffic Management**
   a. No changes – site vehicles continuing to use main carriageway for access throughout Project.

3. **Design**
   a. *Confirmation of changes to pavement design (RFI112A)* – SMEC has issued a formal letter to Hewatt, including an instruction to install SMA10 at 30mm and a request for their comments on the use of the R121 RMS SMA Specification;
   b. *Revised grades of footpaths adjacent to floodway non-conforming (RFI109A/ RFI117A)* – SMEC has issued a response to the RFI to Hewatt.

4. **Construction**
   a. DGB base layer placed from CH1380 and Mirrabei Drive stub and intersection to CH650 – 60% Complete;
   b. Cabling of power supply to the street lights continuing;
   c. Shared path preparation of subgrade continuing and the placement DGB material commenced;
   d. Subsoil drain high end and intermediate riser flushing points being installed;
   e. Pointing of stone pitching at western end of floodway completed;
   f. Construction of equestrian/shared path retaining wall commenced;
   g. Pedestrian handrail installation continuing at the bridge, footings completed on western side and formwork on the eastern side of the bridge constructed;
   h. Preparation of base and mowing strips for decomposed granite under the guard rail at the bridge approaches continuing;
   i. Concrete infill in traffic islands continuing;
   j. Kerb construction completed;
   k. Base cover slabs for plantation sumps installed, aprons to be constructed;
   l. Pram ramps in traffic islands commenced;
   m. Topsoiling and landscape activities continuing.

5. **Meetings**
   a. J. Husband and inspected the stone pitching in the floodway on Tuesday 11/3/14. An additional area was identified and marked up to be removed and replaced.

6. **Issues**
   a. *Raising of Telstra Pit at CH40* – Hewatt plan to get in touch with Telstra to try to push to have the works completed as soon as possible;
   b. *Excess material potentially requiring disposal* – it appears there may be a small amount of material that will not be able to be disposed on-site (<1000m3). This is to be confirmed as finished surface levels are finalised;
   c. *Interface with Springbank Rise (ISS002)* – there are still a few issues to be addressed regarding the interface. A meeting with Lend Lease is being planned for the upcoming week;
   d. *Stone-pitching finishes (ISS022)* – EDD advised that they were not satisfied with the area of stone pitching that was removed by Hewatt. EDD have

marked up an area they wish to be removed by Hewatt. SMEC, Hewatt and ACT Government will meet on Tuesday 18 March 2014 to discuss.

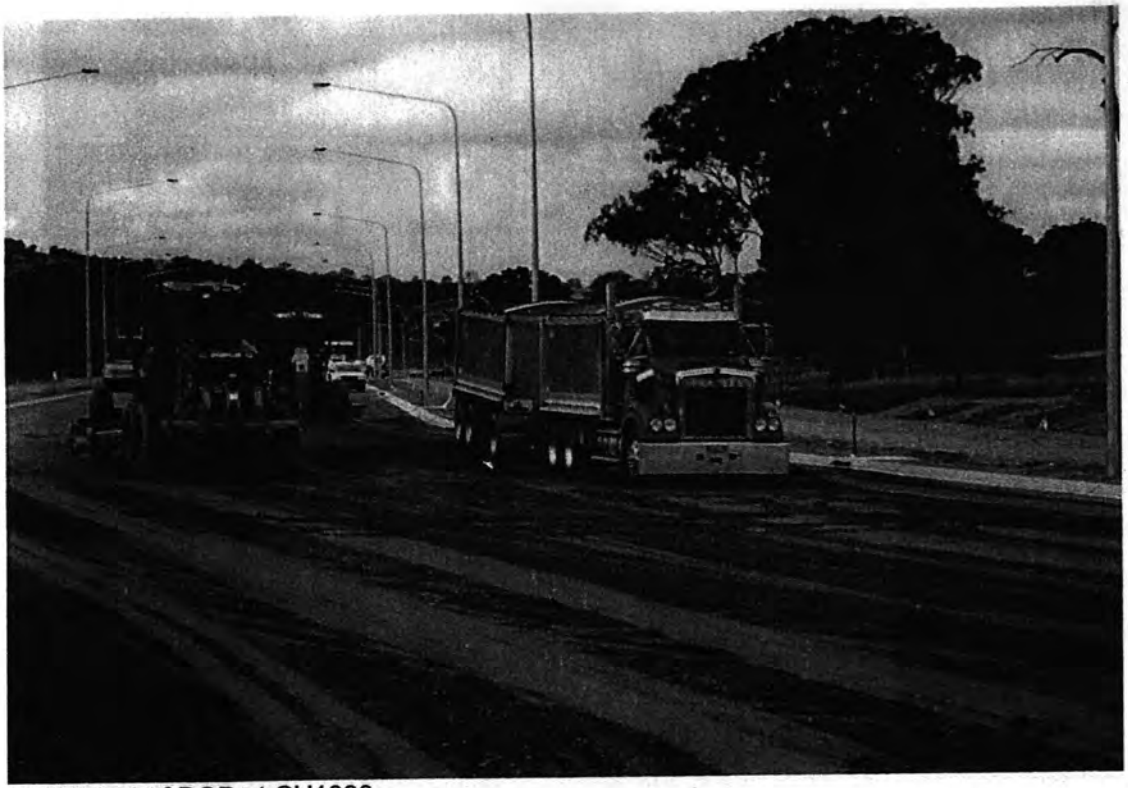
7. **Payments**
   a. SMEC has certified an amount of for Payment Claim No. 11 for February 2014;
   b. Brown are currently completing a review of Imported Fill and General Earthwork Quantities to be used in SMEC's assessment of final earthwork quantities.

8. **Activities Planned for the Upcoming Fortnight**

   a. Continue placement DGB for base course from CH650 to Burrumarra Avenue CH50;
   b. Complete footing for retaining wall between shared and equestrian paths under the bridge and continue construction of the wall;
   c. Landscape activities to continue across site;
   d. Connection of power supply from ActewAGL's mini-pillars to the streetlights and traffic lights;
   e. Continue with placement of DGB for the base of the shared path;
   f. Construction of pram crossings;
   g. Construct flushing points for sub soil high end and intermediate risers;
   h. Pour concrete foot paths and pedestrian fence footings at the eastern side of the bridge;
   i. Complete mowing strips for decomposed granite area at the bridge approaches;
   j. Continue with concrete infill of traffic islands;
   k. Construction of pram crossings continuing.

Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos



Placement of DGB at CH1000

Roller compacting base at CH1080

Preparing the base for decomposed granite around the north west guard rail

Constructing the equestrian/dhared path retaining wall

24

Husband, John

From: Husband, John
Sent: Friday, 14 March 2014 10:59 AM
To: ; Hancock, Patrick
Subject: RE: Horse Park Dr 1

Gents

The decision has been made to use SMA10 on HPD2 and this will apply to the remaining section of HPD1. The current design depth of 40mm is acceptable for SMA10.

Any queries please get back to me

Thanks

John Husband | Senior Project Manager
'hone **02 6205 2920**| Fax 02 6207 6621| Mobile
ıfrastructure and Capital Works| Economic Development Directorate | **ACT Government**
Level 6, TransACT House, 470 Northbourne Ave, Dickson ACT 2602 | GPO Box 158 Canberra ACT 2601 www.edd.act.gov.au

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From: [mailto: @brownconsulting.com.au]
Sent: Thursday, 13 March 2014 10:06 AM
To: Husband, John; Hancock, Patrick
Subject: Fwd: Horse Park Dr 1

John / Patrick

Could you please confirm what SMA type we are to use

Regards

| Senior Engineer
Brown Consulting (ACT) Pty Ltd
Roads & Traffic • Canberra • Australia
P 02 6211 7100 | M

//// This message has been created using touch screen keyboard software. Please excuse any punctuation or typographical errors ///

Begin forwarded message:

> **From:** @hewatt.com.au>
> **Date:** 13 March 2014 9:45:13 am AEDT
> **To:** ' /@brownconsulting.com.au" < @brownconsulting.com.au>
> **Subject: Horse Park Dr 1**
>
> Hi
>
> For Horse Park 2 we have been instructed to change the asphalt thickness from 40 to 30mm, and possibly the mix will change from SMA14 to SMA10.
>
> Can you please confirm with EDD if these changes are to be implemented in Horse Park Dr 1?

Senior Project Engineer

PO Box 94 HALL ACT 2618
M | **T** | **F**
@hewatt.com.au | www.hewatt.com.au

Communication Honesty Accountability Respect Teamwork

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

WEEKLY REPORT FOR WEEK ENDING: 8 March 2014

1. WHS

1. Site inductions continuing;
2. There was a WHS Incident involving a machine operator having his foot run over by a remote control guardrail post driving machine he was operating. The worker missed 3 days of work as a result of the incident. The incident was not a Notifiable Incident.

2. Traffic Management

1. No changes – site vehicles continuing to use main carriageway for access throughout Project.

3. Design

1. *Raising of Telstra Pit at CH40 (RFI104A)* – Telstra's quote has been accepted by Hewatt. Telstra have engaged a sub-contractor to complete the works;
2. *Confirmation of changes to pavement design (RFI112A)* – it was confirmed in Friday's Site Meeting that SMA10 with a 30mm specified layer thickness was to be constructed for HPD2;
3. *Revised grades of footpaths adjacent to floodway non-conforming (RFI109A/ RFI117A)* – Brown has revised the design model again to achieve acceptable grades and cross-falls. The original revision had been produced assuming earthwork levels could not be changed.

4. Construction

1. DGB base layer placed from HPD1 interface to CH1380 and Mirrabei Drive stub and intersection;
2. Street light columns and lanterns installation completed. Cabling of power supply to the street lights to be continuing;
3. Earthworks and setting out of shared path continuing;
4. Subsoil drain high end and intermediate riser flushing points being installed;
5. W-beam guardrail completed;
6. Stone pitching of western end of floodway completed;
7. Pedestrian handrail installation continuing at the bridge, footings completed on western side and commenced on the eastern side of the bridge;
8. Preparation of base and mowing strips for decomposed granite the guard rail at the bridge approaches;
9. Concrete infill in traffic islands continuing;
10. Earthworks and set out for the shared and equestrian paths under the bridge completed and construction of footing for retaining wall 75% complete;
11. Kerb construction completed for carriage way, except for two traffic islands at Road 4;
12. Base cover slabs for R sumps installation – 85 % Complete;
13. Sewer manhole vacuum test completed for S2, S3, S7, S8 and S16;
14. Topsoiling and landscape activities continuing.

5. Meetings

1. Fortnightly Project Site Meeting was held on Friday 7 March 2014.

6. Issues

1. *SMA/Pavement Changes* – confirmation of the revised SMA wearing course details were confirmed in the Fortnightly Site Meeting;
2. *Excess material potentially requiring disposal* – it appears there may be a small amount of material that will not be able to be disposed on-site (<1000m3). This is to be confirmed as finished surface levels are finalised;
3. *Interface with Springbank Rise (ISS002)* – Lend Lease have bitumen spray mulched over Hewatt's backfilled electrical trench within Springbank Rise, therefore Hewatt will not have to complete these works. There are still a few issues to be addressed regarding the interface. A meeting with Lend Lease is being planned.
4. *Stone-pitching finishes (ISS022)* – EDD advised that they were not satisfied with the area of stone pitching that was removed by Hewatt. EDD have marked up an area they wish to be removed by Hewatt. SMEC will issue a notice to Hewatt.

7. Payments

a. The Progress Claim for February 2014 has been submitted by Hewatt and is to the value of

b. SMEC will assess the claim and produce a Payment Certificate.

8. Activities Planned for the Upcoming Fortnight

1. Continue placement DGB for base course from CH1380 to Burramurra Avenue CH50;
2. Complete footing for retaining wall between shared and equestrian paths under the bridge and commence construction of the wall;
3. Landscape activities to continue across site;
4. Connection of power supply from ActewAGL's mini-pillars to the streetlights and traffic lights;
5. Continue with placement of DGB for the base of the shared path ;
6. Complete kerb construction for Road 4 traffic islands;
7. Construct flushing points for sub soil high end and intermediate risers;
8. Commence concrete foot paths and approaches at the bridge;
9. Complete mowing strips for decomposed granite area at the bridge approaches;
10. Continue with concrete infill of traffic islands;
11. Complete installation of pedestrian fence at the bridge.

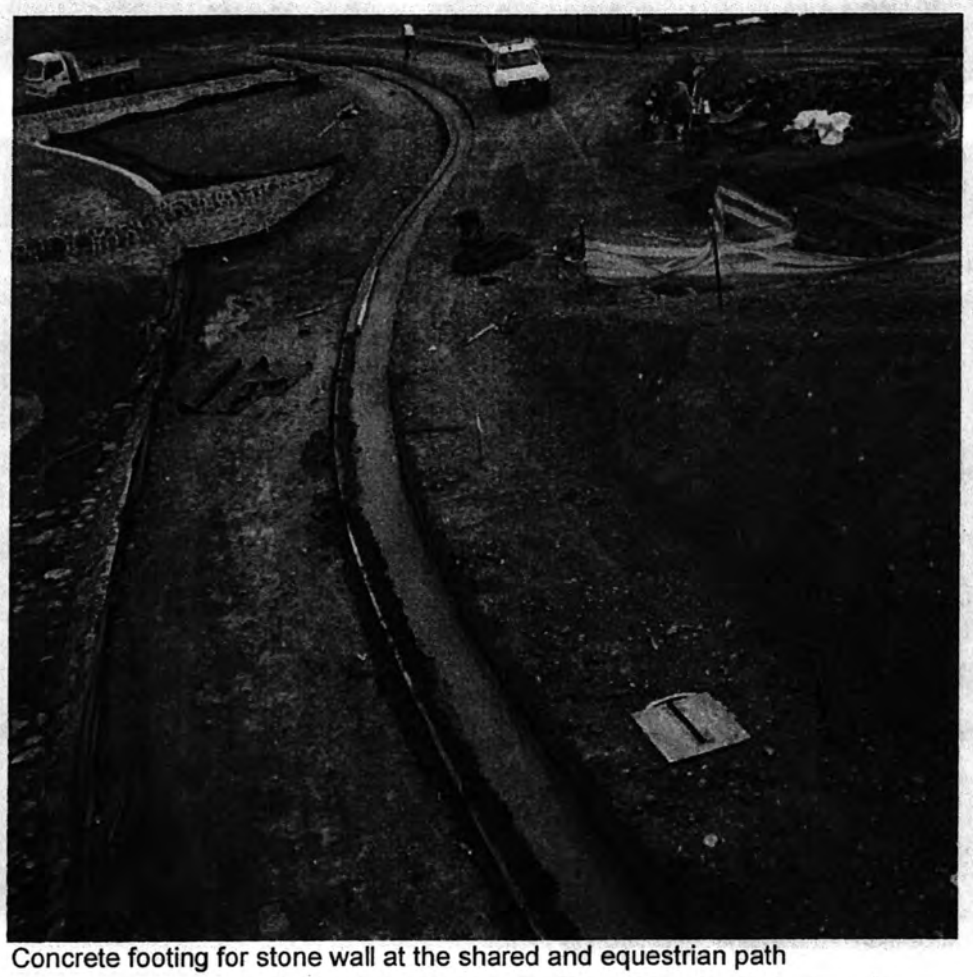
Superintendent's Representative
Horse Park Drive Extension, Burrumarra Ave – Mirrabei Dr
SMEC Australia Pty Ltd
E: @smec.com Ph:

Photos

Driving in guard rail posts at CH520

Placement of DGB at Mirrabei intersection

Placement of DGB on shared path



Concrete footing for stone wall at the shared and equestrian path

HORSE PARK DRIVE EXTENSION
BURRUMARRA AVENUE to MIRRABEI DRIVE

ACT HEWATT SMEC

MEETING: SITE MEETING - MINUTES

LOCATION: Project Site Office, Horse Park Drive, Jacka
DATE: Friday, 7 March 2014
TIME: 10.30am
ATTENDEES:

Economic Development Directorate: Michael McGrath (MM)
John Husband (JH)
Michael Sergi (MS)

Shared Services Procurement: Patrick Hancock (PH)
James Cargill (JC)

Brown:

SMEC:

Hewatt:

No.	Item Description	Action
01.	WHS • There has been a single incident since the last meeting involving the W-beam post driving machine operator running over his foot with the remote controlled tracked machine. Hewatt completed a Hewatt Incident Form for the incident. MM asked whether the incident was a "Notifiable Incident" and needed to be reported to WorkSafe. • A date for the follow up ACT Government WHS Audit has been confirmed for Monday 17 March 2014. It is likely the follow up audit will only be a single day. MM will be an apology.	
02.	Review Previous Minutes / Matters Arising • has submitted a price for works undertaken by Hewatt to provide electricity to the traffic light and streetlight controllers. SMEC is currently assessing the claim. • JH will ask EDD communications if an opening event is going to be held for HPD1&2.	JH
03.	Design • RFIs: 116 submitted; 2 open • noted that SMEC and the designers from Brown met on-site last week to discuss a number of design issues. The purpose of the site visit was to resolve any design issues as soon as possible to avoid having delays in construction. SMEC issued minutes and Brown actioned their items. Responses from the designer were received promptly. A revised 12D design model was issued by Brown. The on-site meeting was valuable. • SMEC issued a response to RFI109A which formed part of the response to the walk-through with Brown. A follow up RFI117A has been submitted by Hewatt to confirm levels and grades of realigned path. • SMA design and specification changes: o JH confirmed that the design of the SMA wearing course layer has been changed to SMA10 with a 30mm	/Brown

	layer thickness. noted that he has been in touch with Downer who advised the per-tonne rate for SMA10 is slightly higher than SMA14. o MM asked if Hewatt would have an objection to using the RMS SMA Specification (R121) for the supply and placement of the SMA wearing course. noted that the RMS Specification may require additional works such as the use of a shuttle-buggy and echelon paving. Hewatt and SMEC will discuss the use of the RMS Specification further. noted that there is not a lot of time until the commencement of SMA placement (noting HPs for mix design are due 10 working days prior to works). o confirmed that the spray seal & prime to be used for the project will be as shown in the design. stressed the importance of Hewatt submitting the HPs for the design of the spray seal & prime to be used for the works. SMEC will need to approve these prior to works taking place. Particular attention will be on the content of cutter oils to be used. o noted that the gap between the completion of spray seal and placement of SMA will likely be 2-3 days. noted that this is pretty standard practice. noted the content of the cutter oil in the spray seal will also have an impact on how soon after the spray seal the SMA can be placed. • Offset of W-beam guardrail behind the kerb o JH raised the issue of where the correct location of the face of the W-beam is, the kerb-line or offset behind the kerb? Noting that HPD1's W-beam is close to the kerb-line, whereas HPD2's is offset. o passed on Brown's advice, that the ACT Design Standards show it is desirable for the W-beam to be offset behind (for speed limits >70kph) to avoid 'minor nuisance vehicle impacts' with the overhang of the car before the wheels have hit the kerb. o MM added that this question may be asked by Asset Acceptance during handover. • MS asked whether there was a requirement behind for no fixed structures to be located behind guardrail (eg. streetlighting). noted that this had been investigated and the standards advise that a clear zone is desirable, but where not possible structures need to be frangible. to forward on reference to MS. • MM asked whether the N2C interface design issues were resolved (relating to the low point on the corner of Burrumarra Ave). advised that this was raised in the walk through with Brown and they advised that these issues have been resolved in a combination of both the HPD2 and N2C's designs. • JH said he had been asked a question about Action Bus Stops along HPD2. PH confirmed that these were mentioned at the start of construction. However Action did not get in quick enough. If the bus stops are to be installed they will be retro-fitted to both HPD1 & HPD2. JH will respond accordingly.	JH
04.	Site Compound • The site compound will be looking to be removed mid-April 2014	

05.	Program • SMA has been pushed back to commence on 31 March 2014. Current programme showing commencement on 19 March 2014 (taking 8 days to complete). • The seal programme is yet to be finalised. Hewatt to confirm whether the seal is to be completed in stages or as a single operation. • MM noted that the PC inspection for HPD2 is to be organised by PH and will include a walk through with EDD. This will be the same as other EDD projects around the Territory. **Current PC Dates** SP1 & SP3 – Civil, Burrumarra Ave – HPD1 – 03 Apr 2014 SP2 & SP4 – Landscape, Burrumarra Ave – HPD1 – 07 Oct 2014	
06.	Environmental • noted that the site has held up well following significant rain events in the past fortnight/month. This is largely due to Hewatt's preparation for these events. • A significant amount of silt has built up in HPD2's floodway that was washed down from Casey landscaping works. Hewatt are to follow this up with (Lend Lease). • There have been no visits from the EPA.	Hewatt
07.	Industrial Relations • Telstra need to be informed early that their subcontractor completing the pit replacement work needs to have necessary ACT IRE Certification.	/Hewatt
08.	Traffic • PH will liaise with Roads ACT to determine how the traffic lights at Mirrabei Dr will be dealt with (on, off, etc.). It was noted that there have been recent talks of accelerating the development of Moncrieff, therefore they may be required.	PH
09.	Community • As noted in Item No. 2, JH will liaise with EDD Communications section regarding potential opening event for HPD1/2. • Hewatt are in correspondence with Lend Lease regarding the topsoiling and bitumen straw mulching of Lend Lease's stockpiles located adjacent to Swale No. 1. Hewatt will be subcontracted to Lend Lease for these works (ie. these works are separate to the HPD2 Contract).	
10.	Staffing • informed the meeting that (HPD2 Foreman/Supervisor) will be moving across to Majura at the start of the week commencing 17 Mar 2014. would send a proposal/note on this and Blake's replacement. o SMEC raised objection to this personnel change. SMEC noted has been on the Project since the start and has done a great job. He will be vital to the successful completion of the Project. SMEC indicated they would respond to proposal accordingly. SSP/EDD supported SMEC's views.	

11.	Utilities • Hewatt have prepared a Handover ITP "schedule" showing plan dates for production of WAE drawings/records. The dates in the sheet will be progressively updated. • to send MM email regarding stormwater handover procedures (the Contract is not up-to-date with respect to this issue). • Water Mains: o Hewatt have received and paid ACTEW Water's quote for connection of HPD2 water main to existing network. o Draft WAE drawings have been produced and SMEC will review, prior to forwarding to ACTEW Water. • NBN Co. is awaiting WAE information/drawings prior to completing final inspection. and will look at getting this information to NBN Co. ASAP to be able to finalise any handover requirements. All survey should be complete. • Hewatt received a quote back from Telstra regarding the pit that needs to be raised at CH40. The quote for the work was for It was agreed this was very high, but there is no other way of carrying out the work. PH will reply to email notifying the quote's approval.	SMEC PH/
12.	Issues • MM noted that the stone pitching that is currently being replaced was not enough. SMEC & EDD will meet following the meeting to discuss. • Handover and WAE for PC (Issue No. 19): o will follow up kerb cracks issues with Asset Acceptance; o SMEC are aiming to have the first revision of the Defects Register issued by the end of next week (Week ending 14/3/14) • Interface with Springbank Rise (ISS002) – following bitumen straw-mulching of Lend Lease's stockpiles, the boundary between the HPD2 and Springbank Rise will not be as clear. For the purposes of handover, a temporary fence will be erected to delineate the boundary. • Springbank Rise Interface (Issue No. 2) was covered in Item 09. Community. • Changes to SMA design (Issue No. 18) was covered in Item 03. Design • Existing Telstra Pit to be raised at CH40 (Issue No. 20) was covered in Item 11. Utilities.	SMEC/EDD Hewatt
13.	EOT • EOT Status (14 submitted): 52.5 Days Claimed; 36.5 Days Granted • EOT No. 14 was submitted by Hewatt for 0.5 day wet weather.	
14.	Variations • **Status** (including BoQ over/underruns) (excl GST): Submitted; 18 Endorsed; .22 Certified for payment (as at end of Jan '14) • MM commended the way that Variations have been promptly dealt with by both the Superintendent and the Contractor. • Approx will be claimed in Variations for the Feb 2014 Claim. SMEC is currently assessing a number of these Variations.	